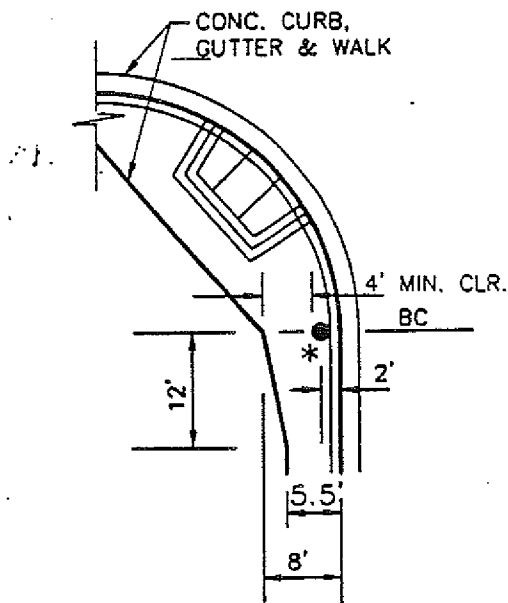
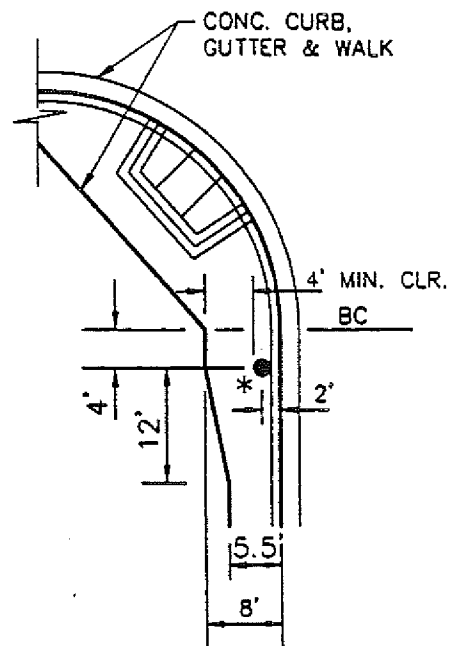


SECTION 9

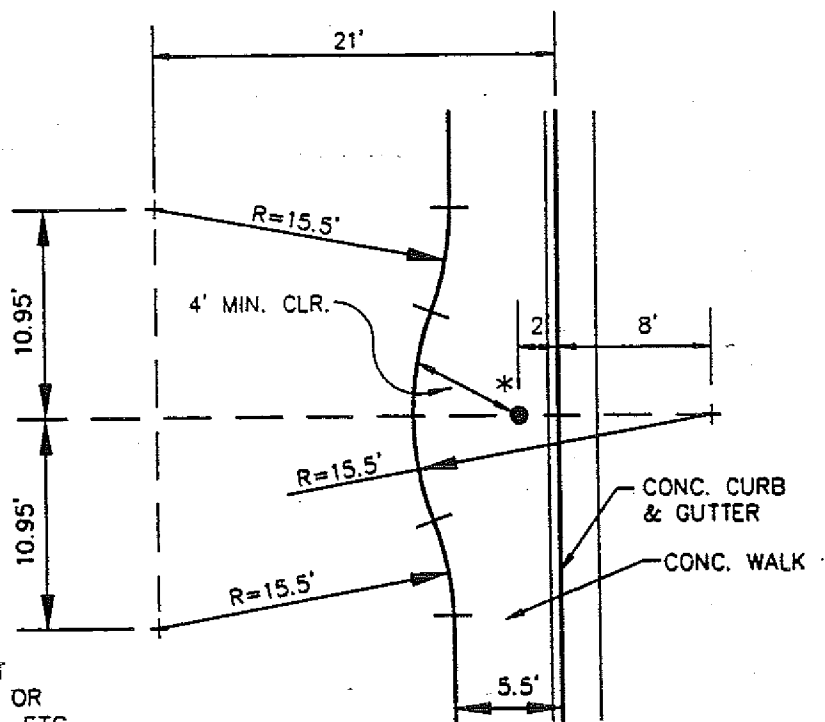
Standard Drawings



STRUCTURE AT BEGINNING OF CURB RETURN



STRUCTURE DISPLACED FROM BEGINNING OF CURB RETURN



* FIRE HYDRANT
UTILITY POLE, OR
LIGHTING STD., ETC.

STRUCTURE ALONG STREET

STD-PLAN
PW-1

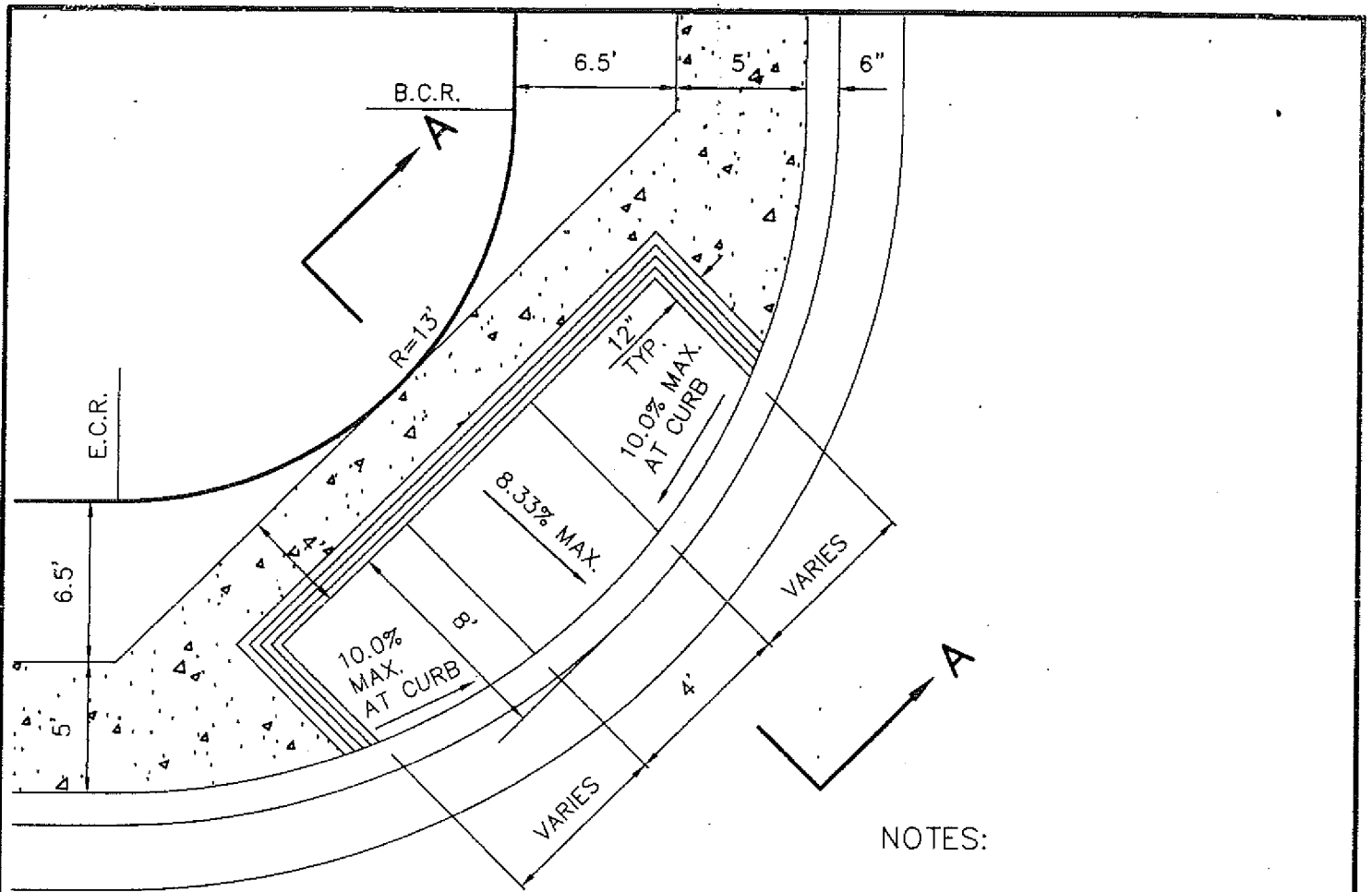
REVISED 7-1-99

SUBMITTED: *Mark R. Huber* 7-9-99
CITY ENGINEER DATE

APPROVED: *John Long* 13 July 99
DIRECTOR OF PUBLIC WORKS DATE

CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS

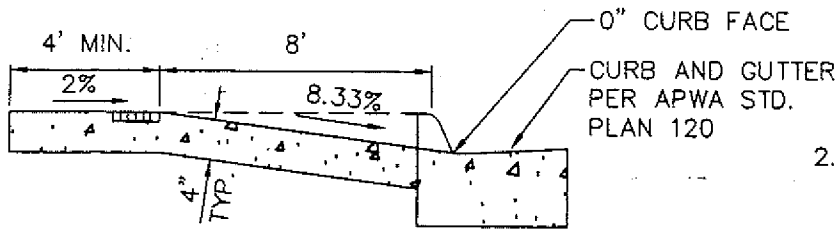
STANDARD BACK OF SIDEWALK
MODIFICATION FOR ABOVE GROUND
OBSTRUCTIONS IN 5' SIDEWALK AREAS



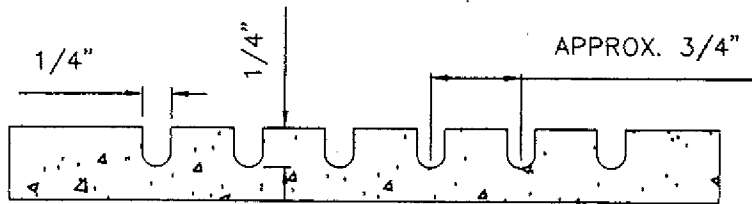
NOTES:

1. THE RAMP SHALL HAVE A 12" WIDE BORDER WITH 1/4" GROOVES APPROXIMATELY 3/4" O.C. "SEE GROOVING DETAIL" THE SURFACE OF RAMP SHALL HAVE A TRANSVERSE BROOMED SURFACE TEXTURE ROUGHER THAN THE SURROUNDING SIDEWALK WHEN LOCATED IN CENTER OF CURB RETURN...
2. RAMP SIDE SLOPE VARIES UNIFORMLY FROM A MAXIMUM OF UP TO 10.0% AT CURB TO CONFORM WITH LONGITUDINAL SIDEWALK SLOPE ADJACENT TO TOP OF THE RAMP.

ALSO SEE SECTION A-A APWA 111-2, SHEET 8 OF 10.



SECTION A-A



GROOVING DETAIL

REVISED 03/05/04

STD. PLAN
PW-2.1

SUBMITTED:

Steve P. Chen 3-9-04
CITY ENGINEER DATE

APPROVED:

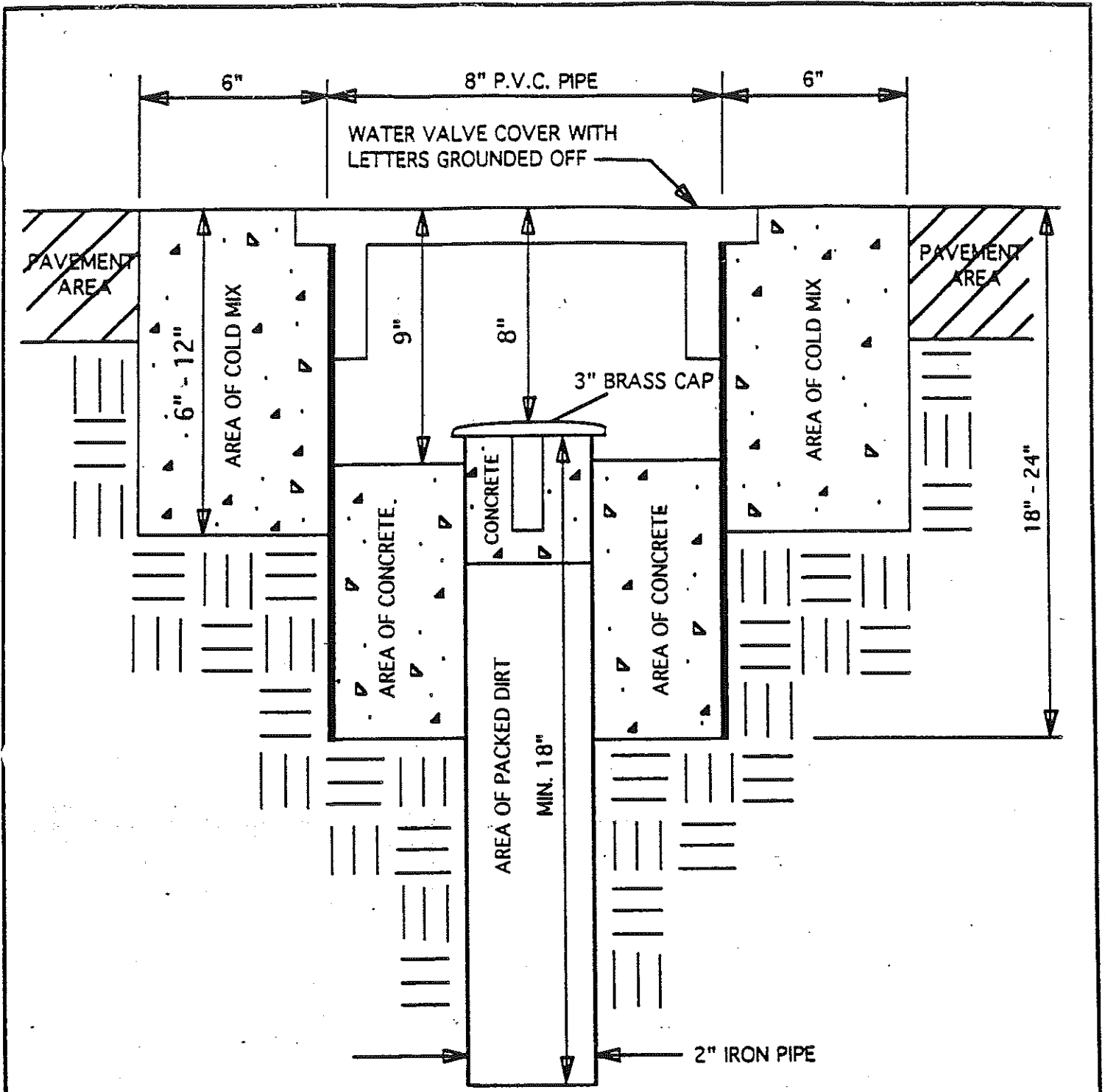
James R. Williams 3/10/04
DIRECTOR OF PUBLIC WORKS DATE

CITY OF LANCASTER

DEPARTMENT OF PUBLIC WORKS

STANDARD HANDICAP RAMP

STREETS WITH 12' PARKWAYS



IF THE GROUND IS DIRT, POUR CONCRETE INSTEAD OF COLD MIX AROUND THE P.V.C. PIPE.

STD. PLAN
PW - 3

SUBMITTED

Paul H. Huber 4-10-96
DATE

CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS

APPROVED

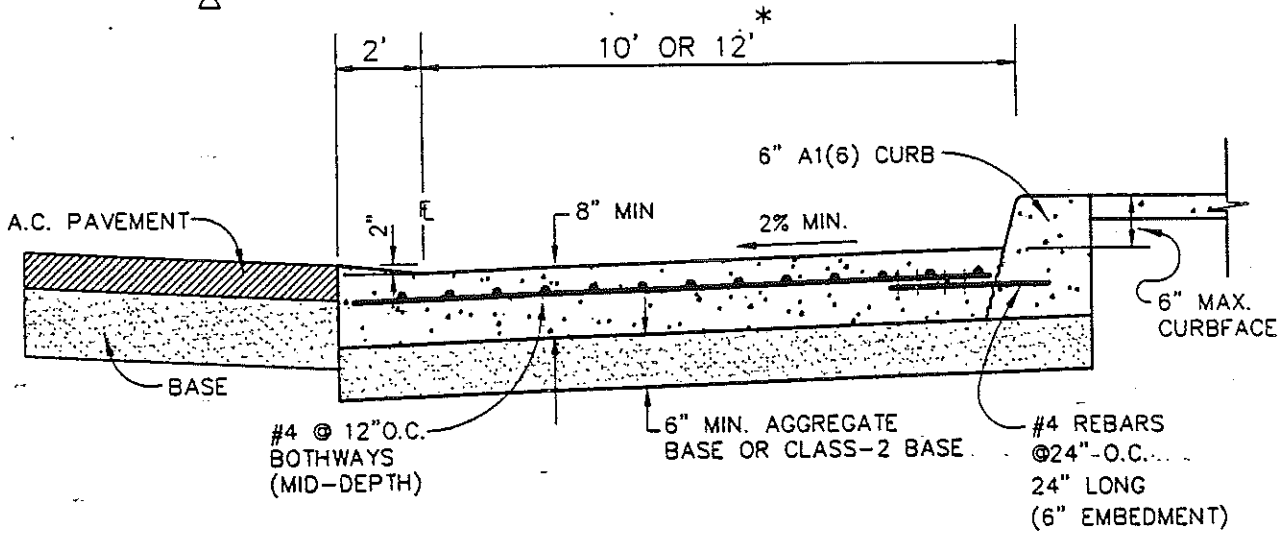
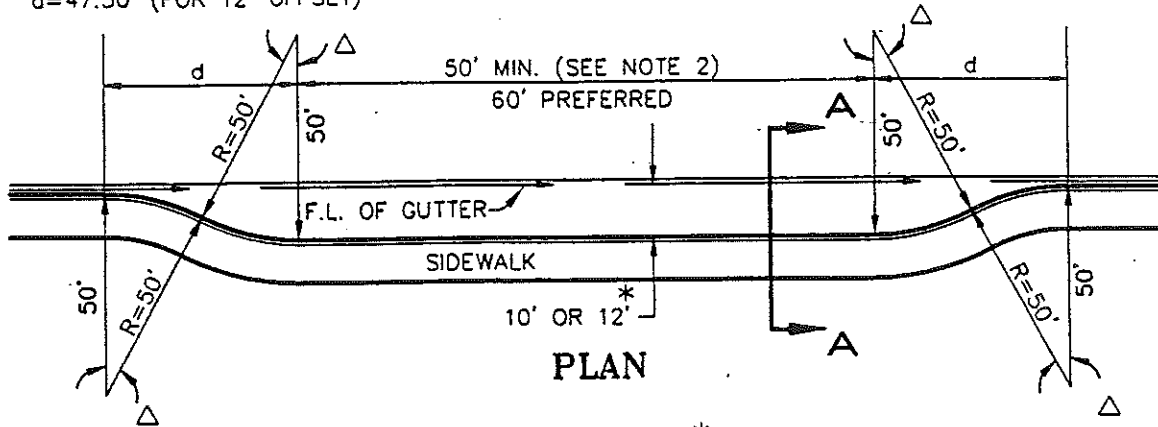
Jeffrey Long 11 April 96
DIRECTOR OF PUBLIC WORKS DATE

MONUMENT IN A HAND HOLE

CURVE DATA (ON CURB LINE)

10' DEEP BAY	12' DEEP BAY
R = 50'	R = 50'
$\Delta = 25^{\circ}50'31''$	$\Delta = 28^{\circ}21'27''$
L = 22.55'	L = 24.75'
T = 11.47'	T = 12.63'

d = 43.59' (FOR 10' OFFSET)
 d = 47.50' (FOR 12' OFFSET)



SECTION A-A

NOTE:

- * 1. BUS TURN-OUT DEPTH SHALL BE 12' WHEN PART OF A RIGHT TURN POCKET
- 2. CONCRETE USED SHALL BE 564C-3000. STEEL SHALL BE 40 GRADE REBARS
- 3. EACH ADDITIONAL PASS THROUGH BUS BERTH ADD 50' AND ANY LAYOVER BERTH ADD 80'

**STD-PLAN
PW-4**

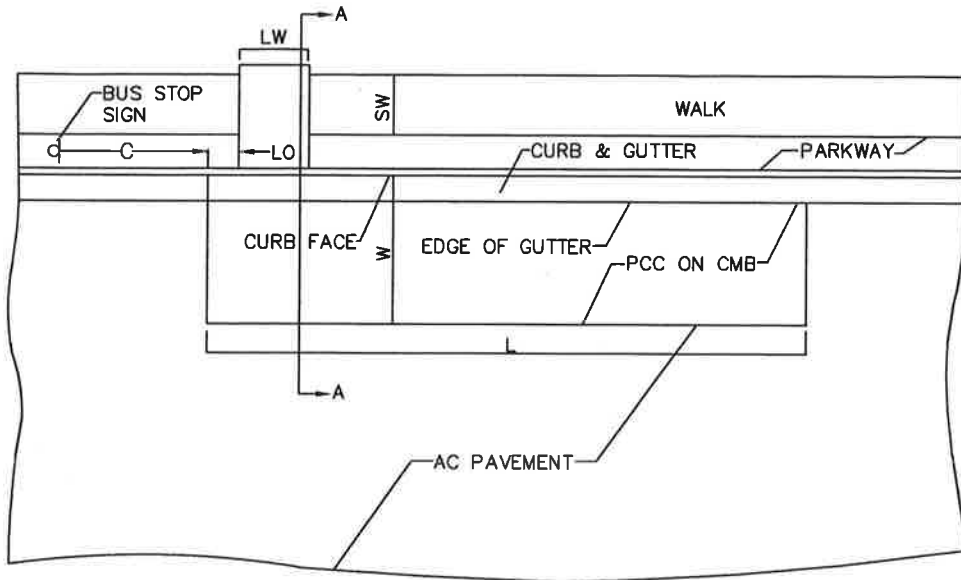
REVISED 7-1-99

SUBMITTED: *Mark R. ...* 7-9-99
 CITY ENGINEER DATE

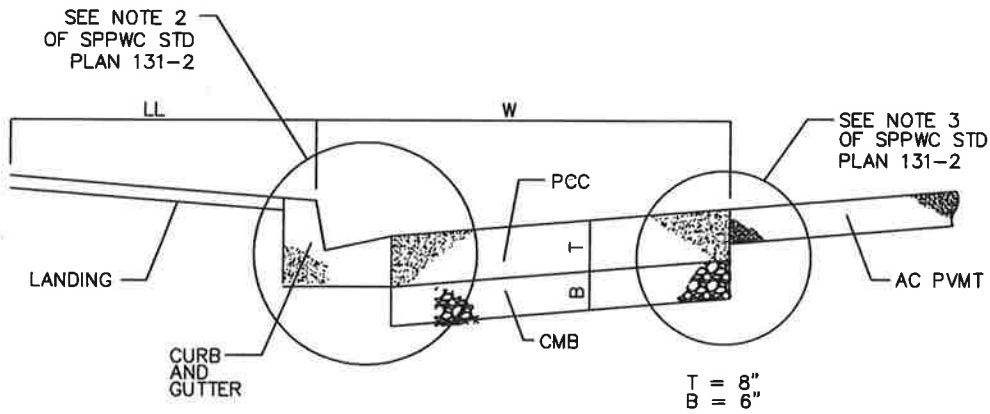
APPROVED: *Jeff H. ...* 7-13-99
 DIRECTOR OF PUBLIC WORKS DATE

**CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS**

BUS TURN-OUT SPECIFICATION



PLAN VIEW



TYPICAL SECTION A-A

NOTES:

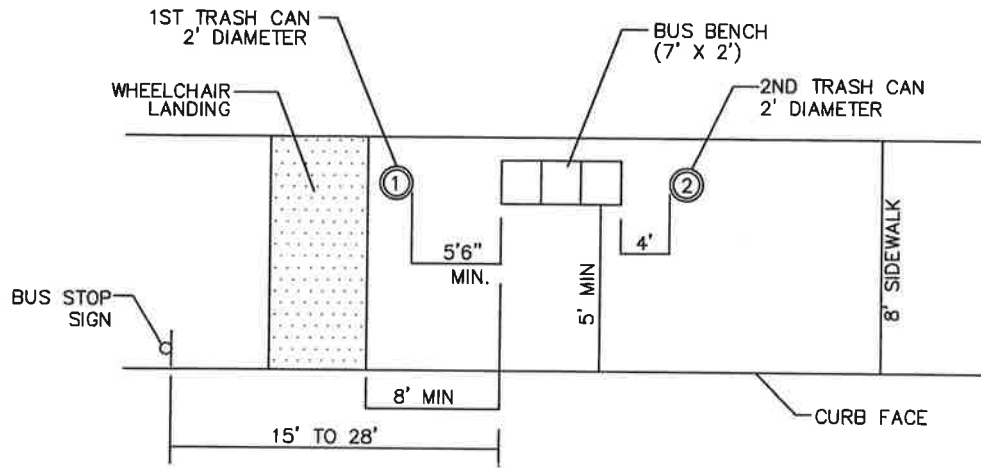
DIMENSIONS (UNLESS OTHERWISE SHOWN):

- | | | |
|-------------|-------------|----------------------|
| L = 50' | SW = 4' MIN | LO = 2' MIN; 15' MAX |
| W = 10' | LW = 5' MIN | LL = 8' MIN |
| C = 10' MAX | | |

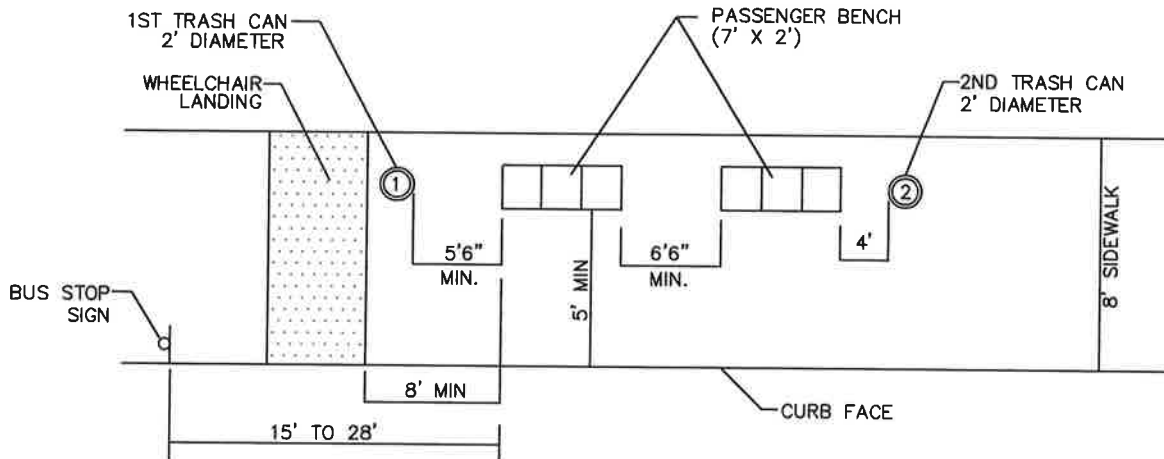
FOR DETAILED SECTION SPECIFICATIONS, REFER TO SPPWC STD PLAN 131-2



SUBMITTED:		DEPARTMENT OF PUBLIC WORKS STANDARD PLAN	
<i>Michelle Cantrell</i> <small>me</small> CITY ENGINEER 1/28/13 DATE			
APPROVED:		CONCRETE BUS PAD	PW 4.1
<i>Robert C. Neal</i> DIRECTOR OF PUBLIC WORKS 1/28/13 DATE			



a.) ONE BUS BENCH
NOT TO SCALE



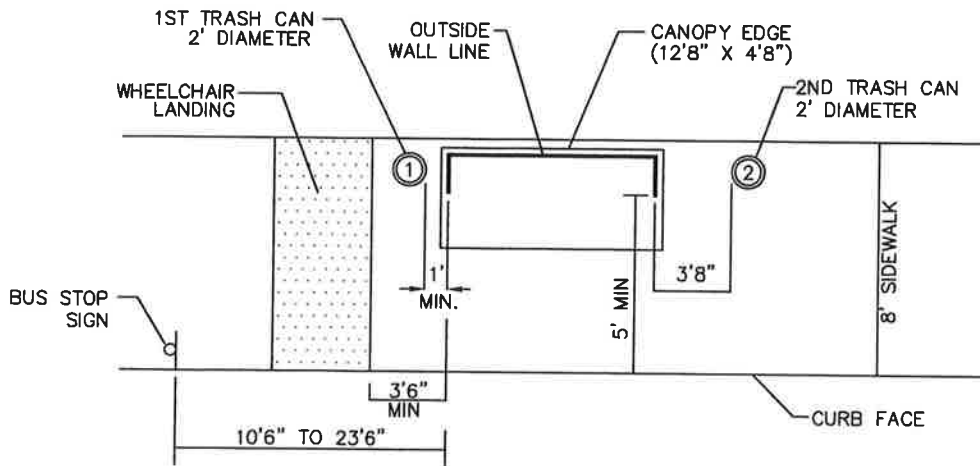
b.) TWO BUS BENCHES
NOT TO SCALE

NOTES:

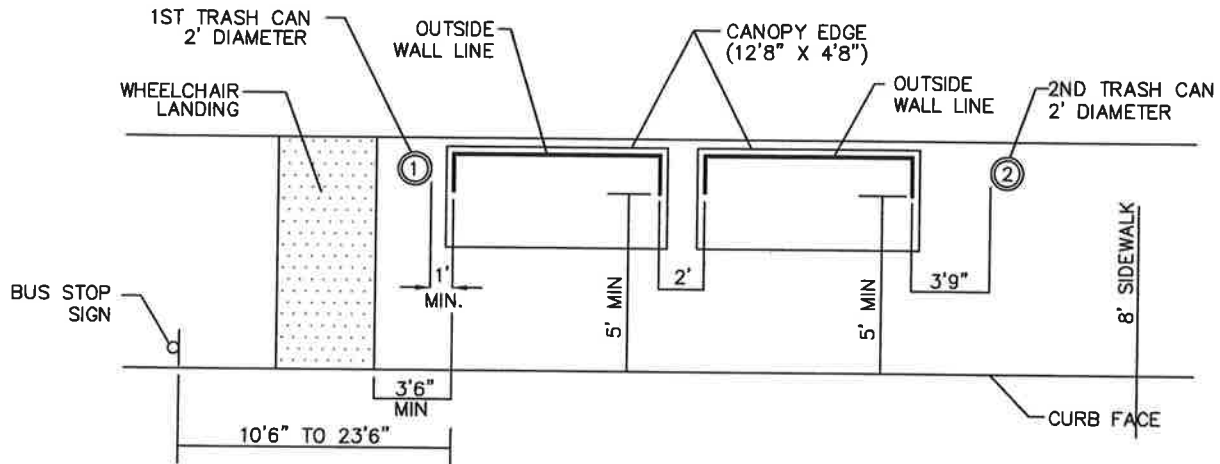
1. THE FIRST BENCH SHALL BE PLACED BETWEEN 15' TO 28' FROM THE BUS STOP SIGN.
2. A TRASH CAN SHALL BE PLACED ON THE SIDE OF THE BENCH, NEAR THE BUS STOP SIGN. A SECOND TRASH CAN, IF ANY, SHALL BE PLACED ON THE OTHER SIDE AS SHOWN.
3. THE WHEELCHAIR LANDING AND THE 5' WIDE PATH TO IT SHALL BE FREE OF ANY OBSTRUCTIONS.
4. INSTALLATIONS SHALL CONFORM TO STANDARD DIMENSIONS UNLESS SPECIFIED OTHERWISE IN ACTUAL PROJECT PLANS.



SUBMITTED: <i>Michelly Cantell</i> CITY ENGINEER		DEPARTMENT OF PUBLIC WORKS	
DATE: 1/28/13		STANDARD PLAN	
APPROVED: <i>Robert C. Neal</i> DIRECTOR OF PUBLIC WORKS		BUS BENCH LAYOUT	PW - 4.2
DATE: 1/28/13			



a.) ONE SHELTER
NOT TO SCALE



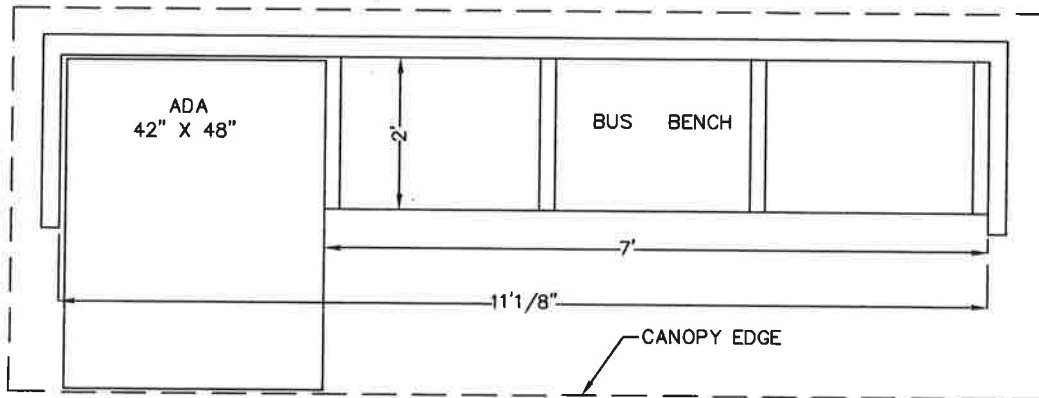
b.) TWO SHELTERS
NOT TO SCALE

NOTES:

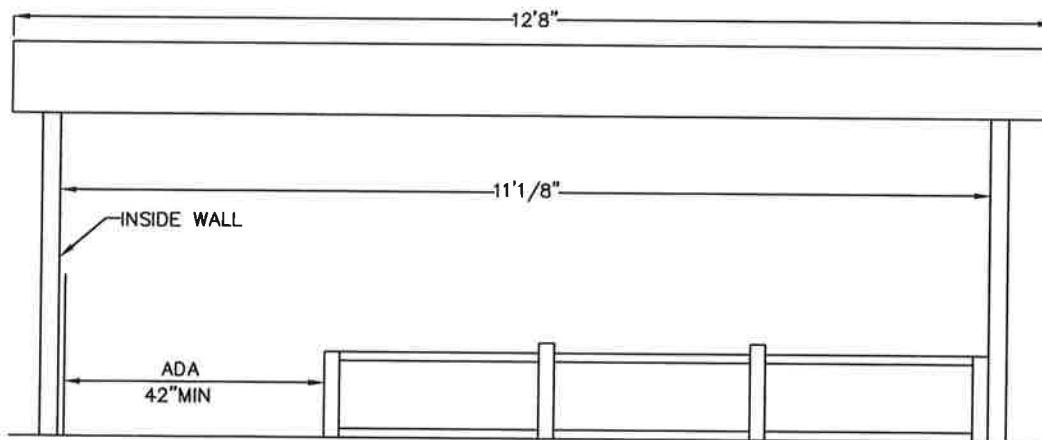
1. THE SHELTER SHALL BE PLACED BETWEEN 10.5' TO 23.5' FROM THE BUS STOP SIGN.
2. THE SHELTER SHALL BE PLACED NEAR BACK OF THE SIDEWALK, WITH A MINIMUM CLEARANCE OF 5' BETWEEN THE SHELTER AND CURB FACE. (THE CANOPY MAY OVERHANG THIS 5' CLEAR AREA.)
3. THE WHEELCHAIR LANDING AND 5' WIDE PATH TO IT SHALL BE FREE OF ANY OBSTRUCTIONS
4. INSTALLATIONS SHALL CONFORM TO STANDARD DIMENSIONS UNLESS SPECIFIED OTHERWISE IN ACTUAL PROJECT PLANS.



SUBMITTED:		DEPARTMENT OF PUBLIC WORKS	
<i>Michelle Cantrell</i>		STANDARD PLAN	
me	CITY ENGINEER	1/28/13	DATE
APPROVED:		BUS STOP SHELTER LAYOUT	
<i>Robert C. Neal</i>		PW - 4.3	
DIRECTOR OF PUBLIC WORKS		1/28/13	DATE



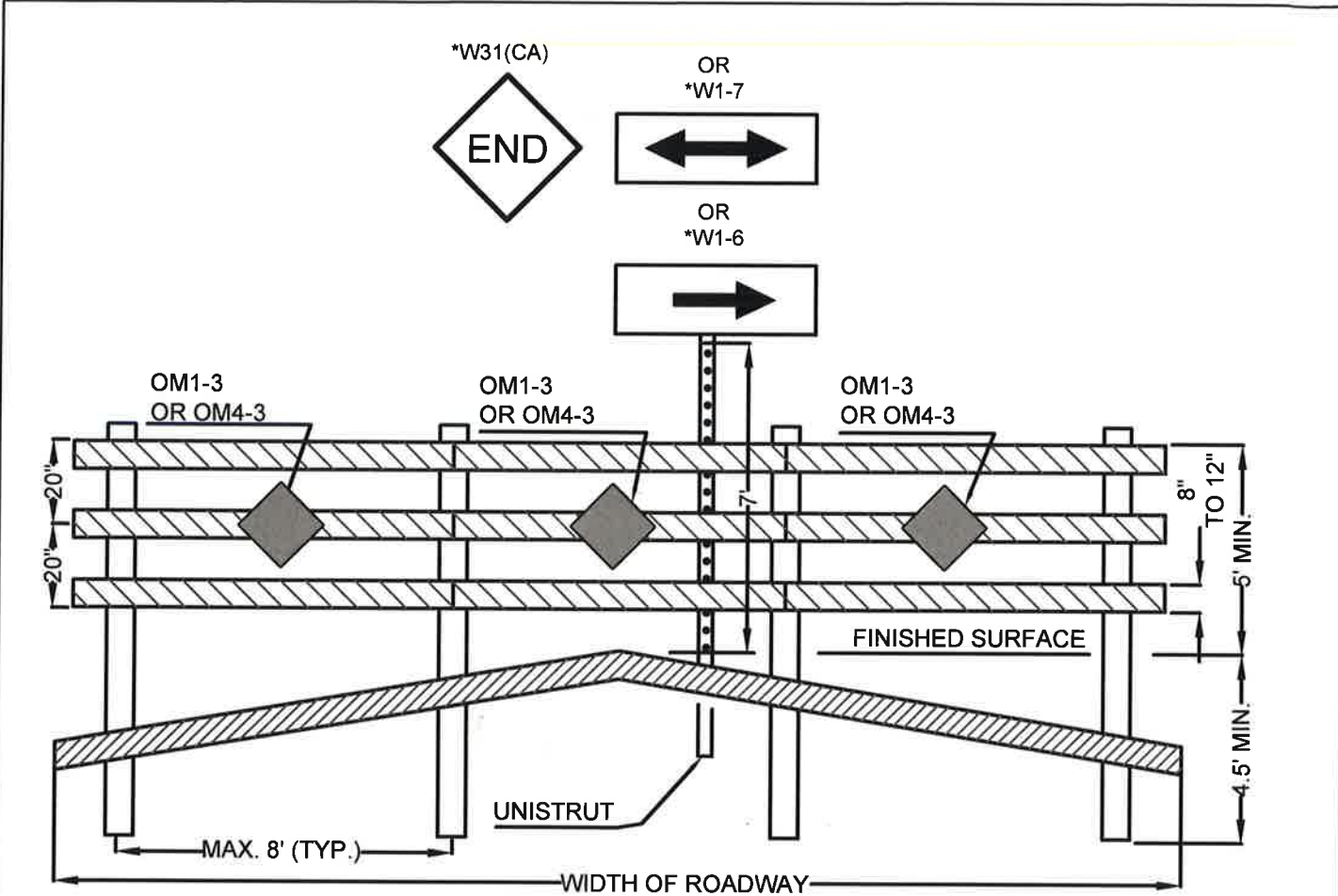
PLAN VIEW
NOT TO SCALE



FRONT ELEVATION
NOT TO SCALE



SUBMITTED: <i>Michelly Cantell</i> <small>me</small> CITY ENGINEER 1/28/13 DATE	DEPARTMENT OF PUBLIC WORKS STANDARD PLAN	
APPROVED: <i>Robert C. Neal</i> DIRECTOR OF PUBLIC WORKS 1/28/13 DATE	BUS BENCH IN SHELTER LAYOUT	PW - 4.4



NOTES:

1. STRIPES ON BARRICADES RAILS SHALL BE ALTERNATING RED AND WHITE 3M HIGH INTENSITY RETROREFLECTIVE STRIPES. THE STRIPES SHALL BE 6 INCHES WIDE. THE STRIPES SHOULD SLOPE DOWNWARD AT AN ANGLE OF 45 DEGREES IN THE DIRECTION TOWARD WHICH TRAFFIC MUST TURN. WHERE BOTH RIGHT AND LEFT TURNS ARE PROVIDED, THE STRIPES MAY SLOPE DOWNWARD IN BOTH DIRECTIONS FROM THE CENTER OF THE BARRICADES. WHERE NO TURNS ARE INTENDED, THE STRIPES SHOULD SLOPE DOWNWARD TOWARD THE CENTER OF THE BARRICADE OR BARRICADES.
2. INSTALL UNISTRUT, W1-6 OR W1-7 PER LANCASTER CITY STANDARD. INSTALL THREE OM1-3 MARKERS ON THE CENTER RAIL WHEN TURNS ARE PERMITTED.
3. INSTALL UNISTRUT & W31(CA) PER LANCASTER CITY STANDARDS. INSTALL THREE OM4-3 MARKERS ON THE CENTER RAIL WHEN NO TURNS ARE PERMITTED.
4. MARKERS SHALL USE 3M HIGH INTENSITY SHEETING.
5. WOOD POSTS SHALL BE 4" x 6", WOOD RAILS SHALL BE 2" THICK AND 8" TO 12" WIDE.

*SEE STREET IMPROVEMENTS PLANS TO DETERMINE PROPER SIGN.



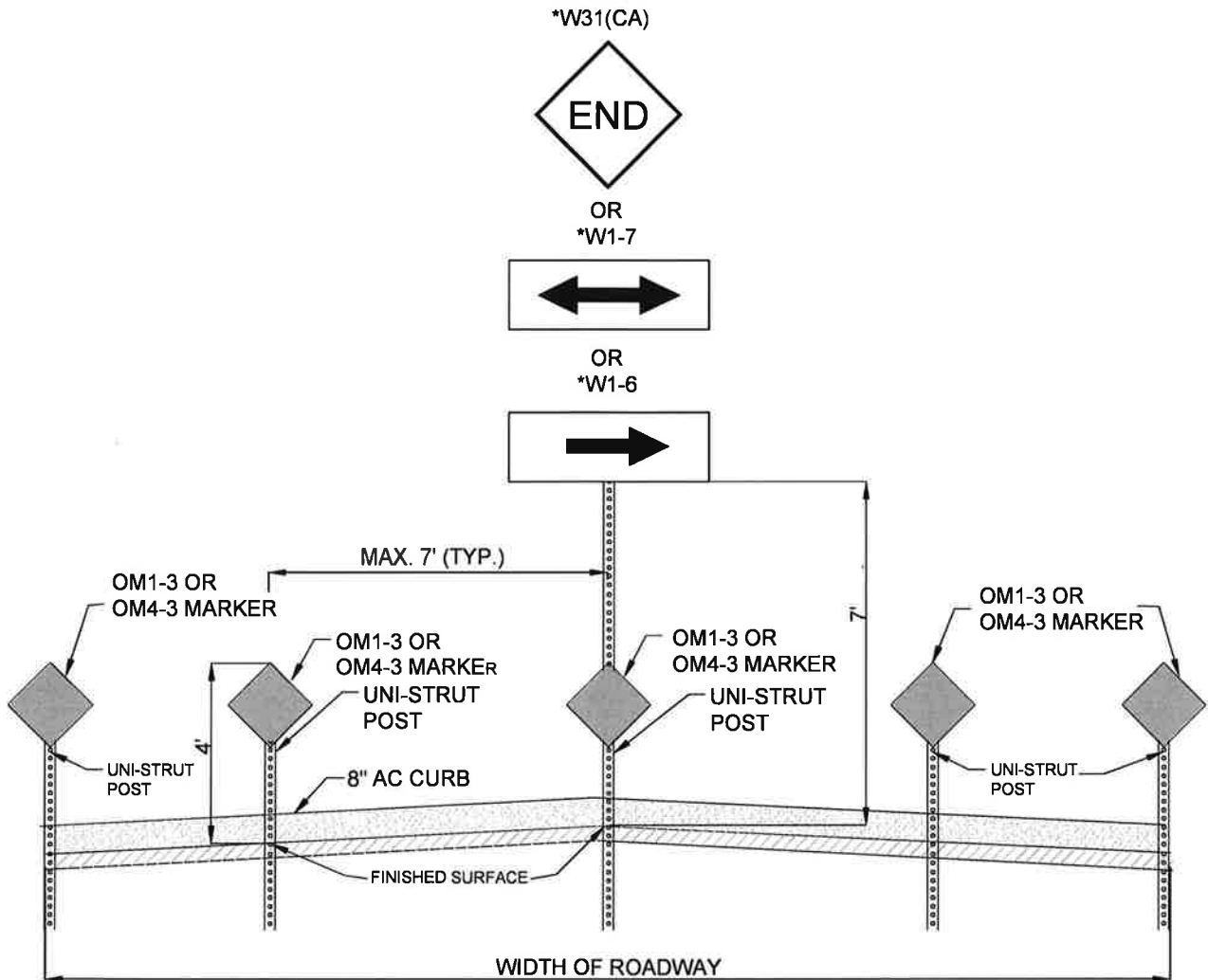
SUBMITTED:
Michelle Cantrell 4/18/16
 CITY ENGINEER DATE

APPROVED:
[Signature] 4/19/16
 DIRECTOR OF DEVELOPMENT SERVICES DATE

DEVELOPMENT SERVICES
 DEPARTMENT
 STANDARD PLAN

END OF ROADWAY
 TYPE III BARRICADE


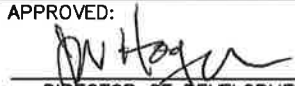
PW-5
 REV 4/15/16



NOTES:

1. MARKERS SHALL USE 3M HIGH INTENSITY PRISMATIC SHEETING.
2. USE OM1-3 MARKERS AND W1-6 OR W1-7 WHEN TURNS ARE PERMITTED.
3. USE OM4-3 & W31(CA) MARKERS WHEN NO TURNS ARE PERMITTED.
4. INSTALL MARKERS AND "UNI-STRUT" POSTS PER CITY OF LANCASTER STD.
5. POSTS AND OM1-3'S SHOULD BE LOCATED AS BEST TO RESTRICT VEHICULAR TRAFFIC ACCESS. POSTS MAY NEED TO BE ADDED AND CAN BE ADDED UP TO THE EDGE OF R.O.W.
6. ADD 8" AC CURB AS SHOWN PER SPPWCP 120-2. CURB MUST NOT IMPEDE THE FLOW OF EXISTING DRAINAGE

*SEE STREET IMPROVEMENTS PLANS TO DETERMINE PROPER SIGN.

SUBMITTED:  CITY ENGINEER 1/3/2020 DATE	DEVELOPMENT SERVICES DEPARTMENT STANDARD PLAN	
APPROVED:  DIRECTOR OF DEVELOPMENT SERVICES 1/3/2020 DATE	END OF LOCAL ROADWAY BARRICADE	PW-6 REV 01/02/20

REMOVE AND RECONSTRUCT
EXISTING CURB AND GUTTER

EX. NAT. GRADE
OR SIDEWALK

#3 REBAR DOWELS
@12" O.C. (12" LONG)
FOR CASES OF DRIVEWAY
APPROACH SLAB WIDENING
(6" MIN. EMBEDMENT BOTH
SIDES OF SPLICE).

#3 REBAR DOWELS
@24" O.C. (18" LONG)

SAWCUT EXISTING GUTTER
3" FROM FLOW LINE

EX. AC PAVEMENT

SECURE #3 BARS INTO DRILLED
HOLE WITH EPOXY (9" MIN. EMBEDMENT)

DRIVEWAY, CURB REPLACEMENT,
AND / OR PARKWAY DRAIN SHALL
BE CONSTRUCTED PER CURRENT
APWA STD. PLANS

STD-PLAN
PW-7

REVISED 7-1-99

SUBMITTED:

W. K. Hedden
CITY ENGINEER

7-9-99
DATE

APPROVED:

J. H. Long
DIRECTOR OF PUBLIC WORKS

7-13-99
DATE

CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS

DRIVEWAY, CURB REPLACEMENT,
AND / OR PARKWAY DRAIN
RETROFIT DETAIL

FIGURE 1
FOUR-LANE STREET WITH TURN
LANE AT INTERSECTION

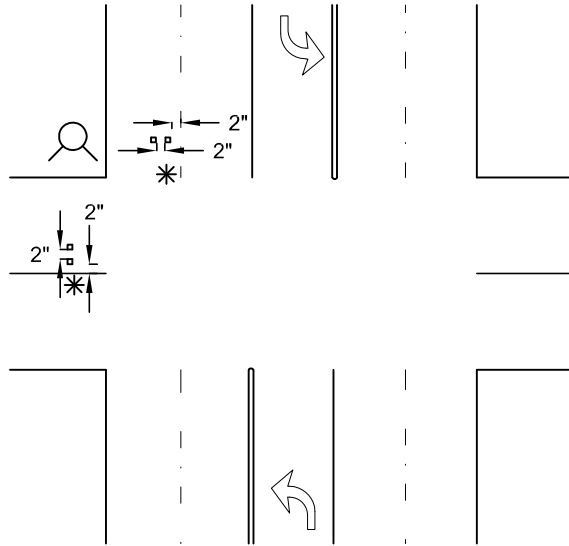


FIGURE 2
AT INTERSECTION

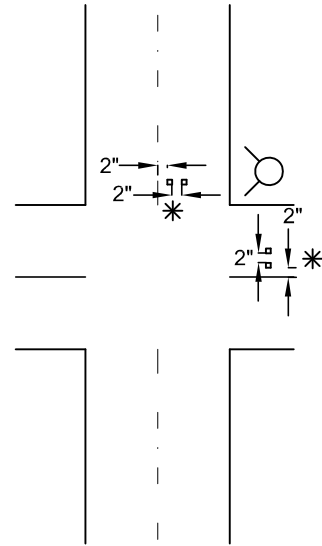


FIGURE 3
TWO LANE STREET

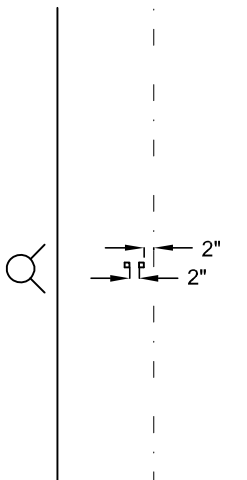


FIGURE 4
MULTI-LANE STREET

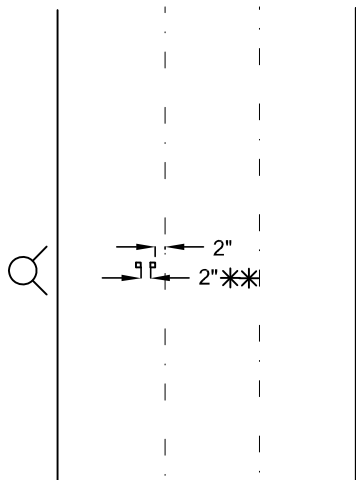
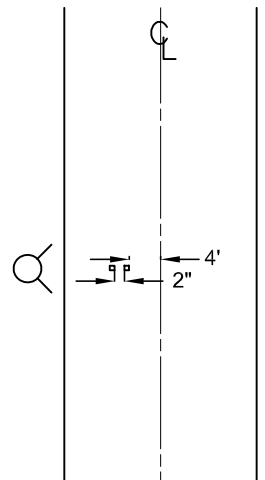




FIGURE 5
STREET WITHOUT
LANE STRIPING



-  = FIRE HYDRANT OR WARF HEAD
-  = BLUE PAVEMENT MARKERS (2 TYP) FOR HYDRANT (1 TYP) FRO WARF HEAD
- * = PAVEMENT MARKERS SHALL BE MIN. 1' OUTSIDE OF CROSSWALK
- ** = PAVEMENT MARKERS SHALL BE IN LANE NEAREST THE CURB / EDGE OF PAVEMENT

CITY OF LANCASTER

DEVELOPMENT SERVICES
DEPARTMENT
STANDARD PLAN

SUBMITTED:

[Signature]

5/22/2020

CITY ENGINEER

DATE

APPROVED:

[Signature]

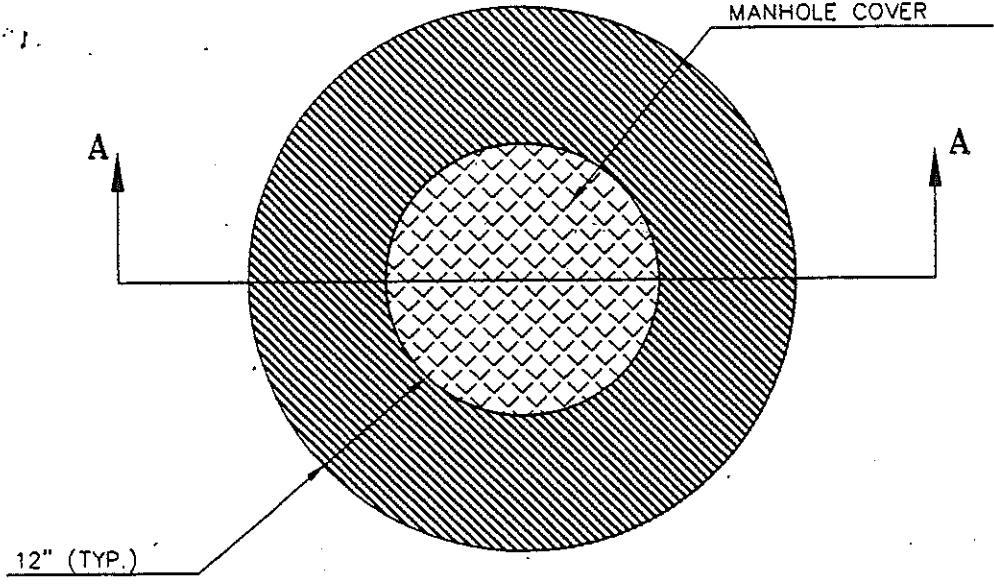
06/01/20

DIRECTOR OF DEVELOPMENT SERVICES

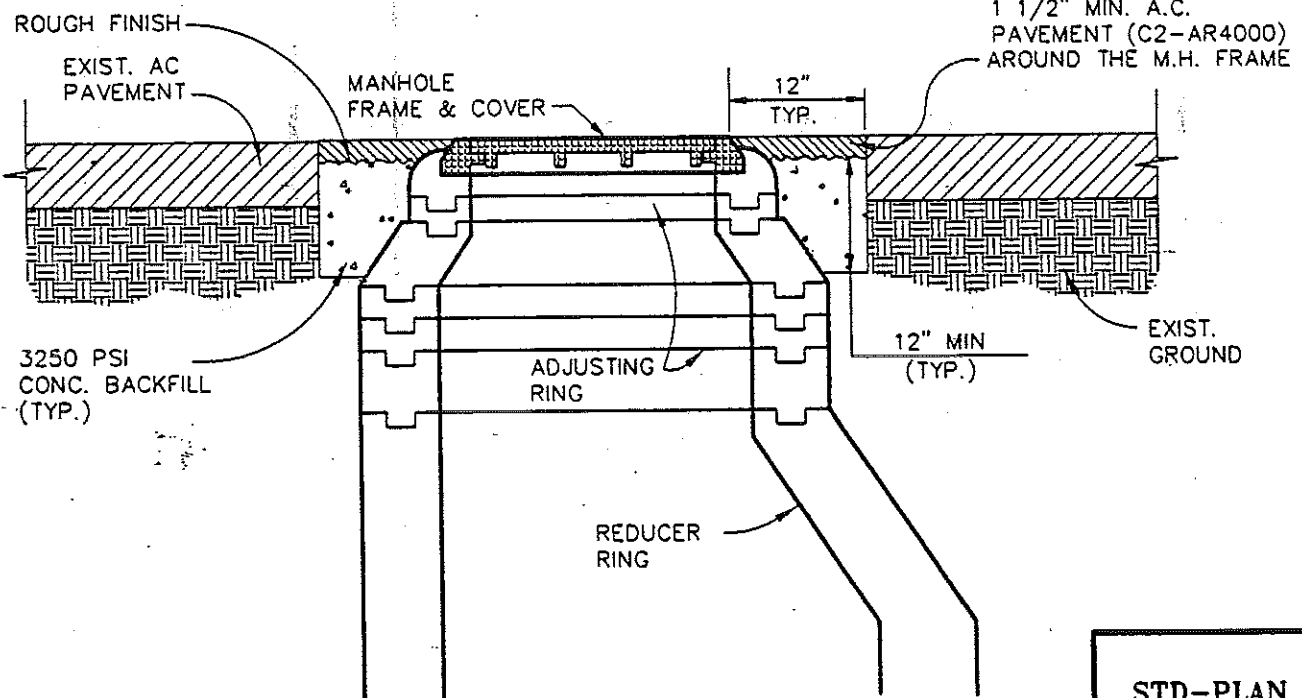
DATE

TYPICAL BLUE HYDRANT
MARKER LOCATION

PW-8
REV 05/20/20



PLAN VIEW



SECTION A-A

STD-PLAN
PW-9

SUBMITTED: *Mark H. Hudson* 7-9-99
CITY ENGINEER DATE

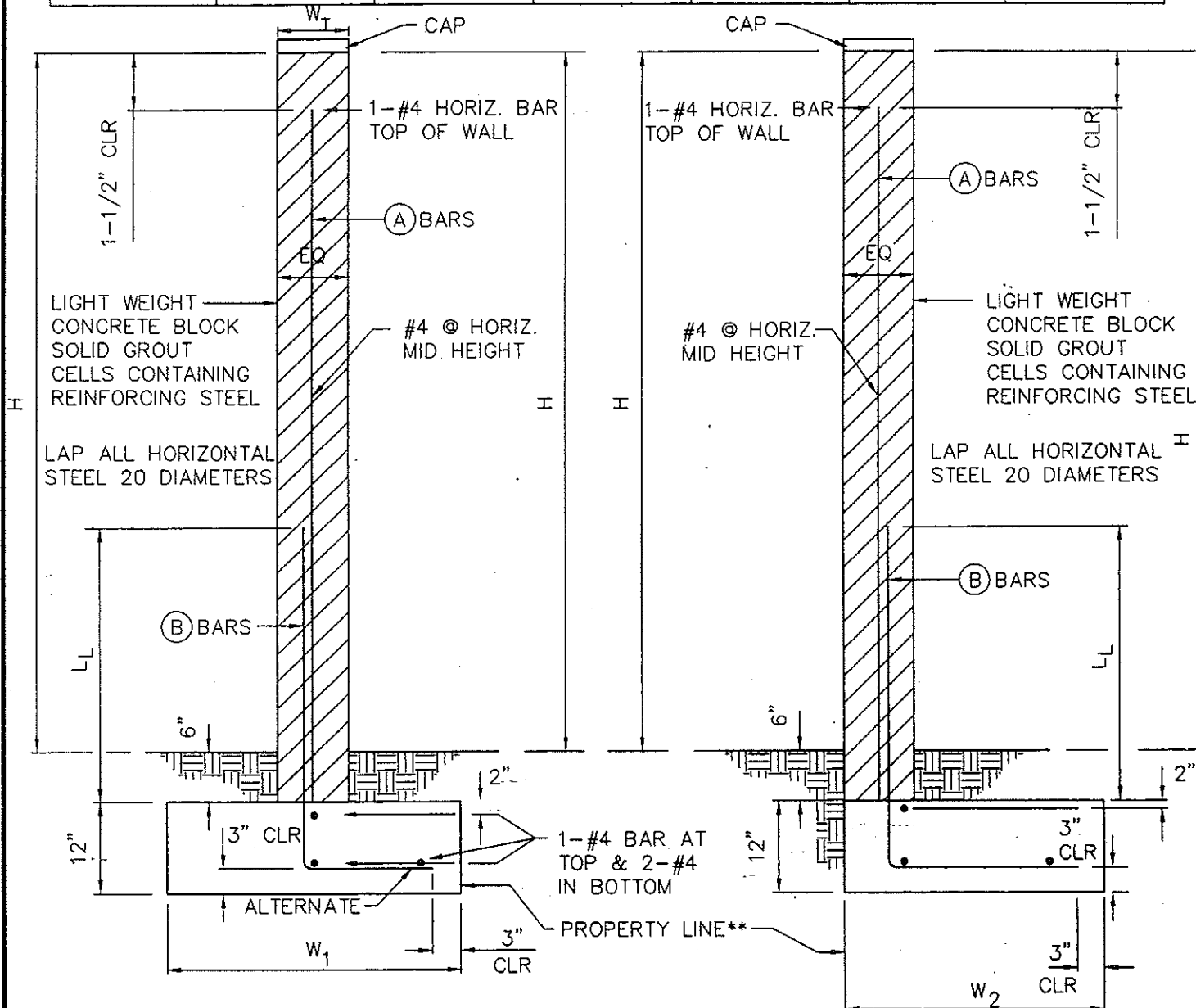
CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS

APPROVED: *Jeff Long* 13 Feb 99
DIRECTOR OF PUBLIC WORKS DATE

MANHOLE ADJUSTMENT TO
FINISH PAVEMENT GRADE

TABLE OF REINFORCING STEEL AND DIMENSIONS

H	W ₁	W ₂	W _T	L _L	(A)	(B)
5'-0"	2'-0"	2'-6"	6"	2'-0"	#4@32"	#4@32"
6'-0"	2'-4"	3'-0"	6"	2'-0"	#4@24"	#4@24"



REVISED 03/11/04

** SHOULD BOTH PRIVATE OWNERS AGREE, THE WALL MAY BE ON PROPERTY LINE.
 6' WALL SHALL BE USED FOR WALLS ADJACENT TO THE CITY LANDSCAPE MAINTENANCE DISTRICTS AND SHALL BE PLACED ON PUBLIC PROPERTY OR LMD EASEMENT.

STD-PLAN
 PW-10

SUBMITTED:

Alvin J. Chen

CITY ENGINEER

3-11-04

DATE

CITY OF LANCASTER

DEPARTMENT OF PUBLIC WORKS

APPROVED:

James H. Williams

DIRECTOR OF PUBLIC WORKS

3/15/04

DATE

FREE STANDING CONCRETE

BLOCK WALL STANDARD

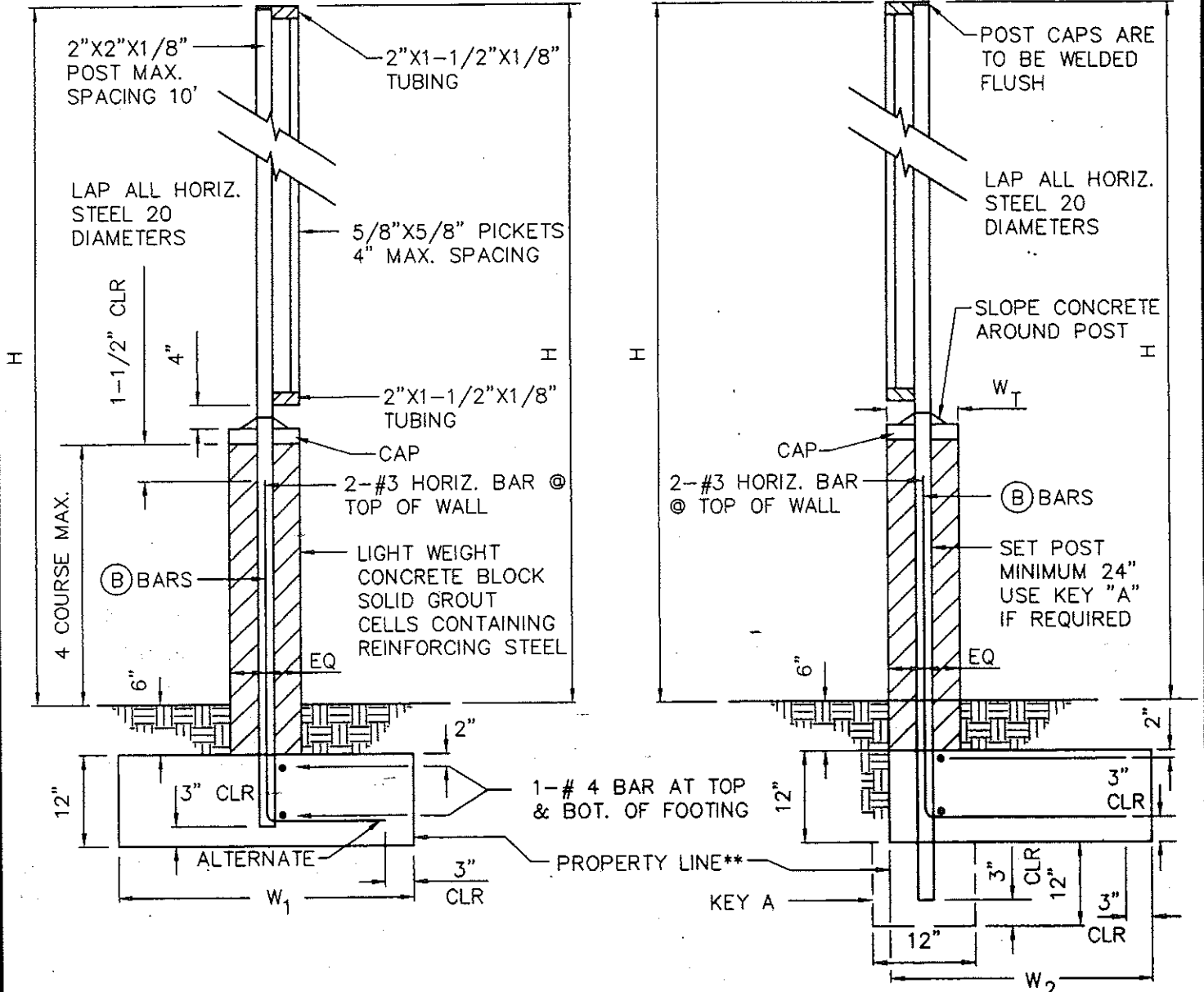
TABLE OF REINFORCING STEEL AND DIMENSIONS

H	W ₁	W ₂	W _T	ⓑ
5'-0"	2'-0"	2'-6"	6"	#4@32"
6'-0"	2'-4"	3'-0"	6"	#4@24"

NOTE:

- 5'-6' EXCEPT FOR WALLS ADJACENT TO LANDSCAPE MAINTENANCE DISTRICT & AROUND DRAINAGE BASINS. IN LATTER SITUATIONS USE 6' OVERALL HEIGHT.
- PLACE HORIZONTAL BARS IN BOND BEAM BLOCK.

ALL WROUGHT IRON TO HAVE 1 COAT PRIMER & 2 COATS BLACK FINISH PAINT



REVISED 03/11/04

** SHOULD BOTH PRIVATE OWNERS AGREE, THE WALL MAY BE ON PROPERTY LINE. 6' WALL SHALL BE USED FOR WALLS ADJACENT TO THE CITY LANDSCAPE MAINTENANCE DISTRICTS AND SHALL BE PLACED ON PUBLIC PROPERTY OR LMD EASEMENT.

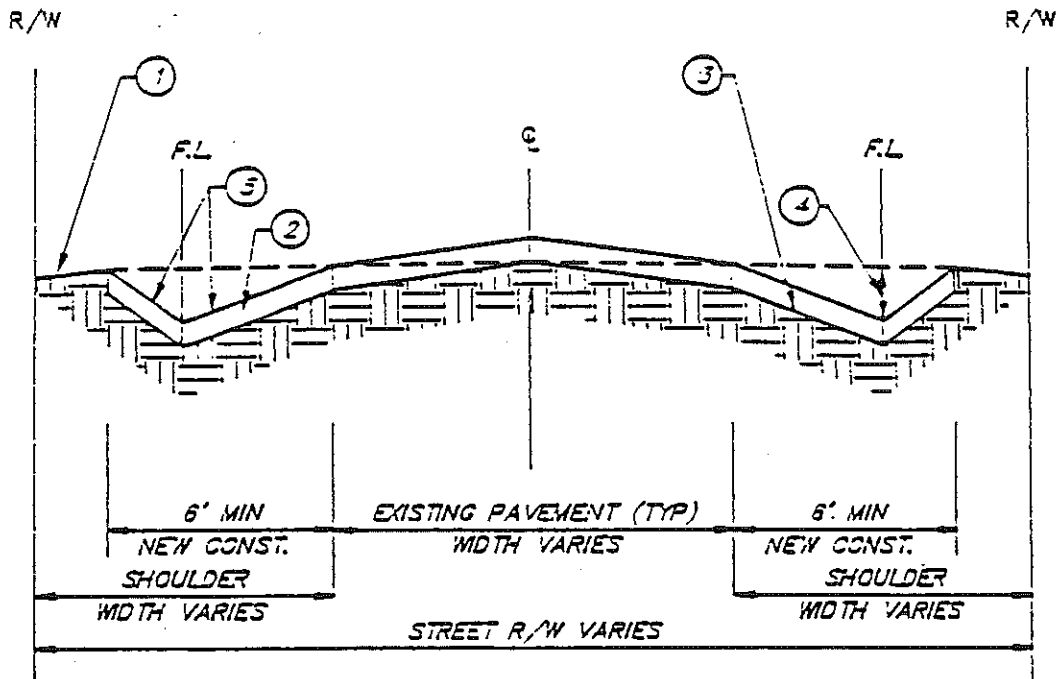
STD-PLAN
PW-11

SUBMITTED:
[Signature] 3-11-04
CITY ENGINEER DATE

APPROVED:
[Signature] 3/15/04
DIRECTOR OF PUBLIC WORKS DATE

CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS
COMBINATION FREE STANDING BLOCK WALL
AND WROUGHT IRON FENCE STANDARD

MODIFIED
MINIMUM TYPICAL SECTION
WHERE REQUIRED
FOR DRAINAGE PURPOSES



- ① MAX 2:1 SLOPE (TYP)
- ② PAVED SWALE WITH BERM (TYP)
- ③ STRUCTURAL SECTION SUBJECT TO APPROVAL BY CITY ENGINEER.
- ④ DEPTH OF SWALE SUBJECT TO APPROVAL BY CITY ENGINEER
- ⑤ MAX 3:1 SLOPE (TYP)

CITY OF LANCASTER

MODIFIED LACRD STANDARD 04-02

PW-12

APPROVED *W. H. Harker* Dec. 6, 1993
CITY ENGINEER DATE

SIGN CODE	DESCRIPTION	STANDARD SIZE
R1	STOP	30"x30"& 36"x36"
R1-2	YIELD	36"
R1-4	ALL WAY	6"x18"
R2	SPEED LIMIT	24"x30"
R7	KEEP RIGHT	24"x30"
R10-1	ONE WAY	36"x12"
R10A	ONE WAY	18"x24"
R11	DO NOT ENTER	36"x36"
R11A-1	WRONG WAY	36"x21"
R13	NO TURN ON RED	24"x30"
R15	NO TURNS	24"x24"
R16	NO RIGHT TURN	24"x24"
R17	NO LEFT TURN	24"x24"
R18-1	RIGHT LANE MUST TURN RIGHT	20"x32"
R18-2	RIGHT LANE MUST TURN RIGHT	36"x36"
R26	NO PARKING ANY TIME	12"x18"
R26D	NO PARKING	24"x24"
R26E	* OFF PAVEMENT	12"Xvar
R26(S)	NO STOPPING ANT TIME	12"x18"
R28(LT.or RT.)	NO PARKING ANY TIME (ARROW)	12"x18"
R28(S)(LT.or RT.)	NO STOPPING ANT TIME (ARROW)	12"x18"
R30	NO PARKING (TIME)	12"x18"
R30A	NO PARKING (TIME)(ARROW)	12"x18"
R32	2 HOUR PARKING (TIME)	12"x18"
R34	NO U TURN	24"x24"
R34-2	NO LEFT OR U TURN	24"x24"
R49	NO PED CROSSING	42"x18"
R59	TURN ONLY	42"x48"
R62D	PED PUSH BUTTON	5"x7.5"
R73-1	LEFT TURN	24"x20"
R73-2	LEFT-U TURN	36"x36"
R73-3	LEFT TURN NO U TURN	36"x36"
R73-5	DUAL LEFT TURN AND U TURN	36"x36"
R73-6	DUAL LEFT TURN NO U TURN	36"x36"
R81	BIKE LANE	24"x18"
R81A	BEGIN	12"x5"
R81B	END	8"x5"
R90(LT.or RT.)	STOP HERE ON RED (ARROW)	24"x36"
R96	NO PEDS	24"x24"
R99	HANDICAPPED PARKING ONLY	12"x18"
R99A	VAN ACCESSIBLE	12"x8"
R100A	TOW-A-WAY IN HCP	12"x8"
R100B	HANDICAPPED TOW-A-WAY	18"x24"
G60-2	PARKING	30"x24"

STD-PLAN
PW-13
SHT. 1 OF 2

SUBMITTED: Bob Wittler 8/12/99
TRAFFIC ENGINEERING MANAGER DATE

CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS

APPROVED: Jeff Long 12 Aug 99
DIRECTOR OF PUBLIC WORKS DATE

STANDARD TRAFFIC CONTROL
SIGNS

SIGN CODE	DESCRIPTION	STANDARD SIZE
G93	BIKE ROUTE	12"x8"
G93A	BEGIN	12"x5"
G93B	END	8"x5"
G95	PARK AND RIDE	24"x30"
W1 (LT.or RT)	CURVE IN ROAD	36"x36"
W2 (LT.or RT)	CURVE IN ROAD	36"x36"
W3 (LT.or RT)	CORNER	36"x36"
W5 (LT.or RT)	CURVE	36"x36"
W6	ADVISORY SPEED	24"x24"
W7	T INTERSECTION	36"x36"
W7A (LT.or RT)	T INTERSECTION	36"x36"
W10	INTERSECTION BY TRACKS	36"x36"
W11	LANE DROP	36"x36"
W17	STOP AHEAD	36"x36"
W19	PAVEMENT ENDS	42"x42"
W31-2.1	END	30"x30"
W31A	ROAD ENDS (FEET)	30"x30"
W32	DIP	36"x36"
W37	BUMP	36"x36"
W41	SIGNAL AHEAD	36"x36"
W42	SLIPPERY WHEN WET	36"x36"
W47	RAIL ROAD CROSSING	30" Dia.
W48	# OF TRACKS	30"x24"
W53	NOT A THROUGH STREET	36"x36"
W53A	NO OUTLET	30"x30"
W53A	NO OUTLET (BLADE TYPE)	24"x6"
W54	PED CROSSING	36"x36"
W54A	ADV PED CROSSING	36"x36"
W56	DOUBLE HEAD ARROW	36"x18"
W57(LT.or RT.)	SINGLE HEAD ARROW	36"x18"
W58	DOUBLE ARROW	30"x30"
W63	ADV SCHOOL XING	36"x36"
W66	SCHOOL XING	36"x36"
W74	THRU TRAFFIC MERGE LEFT	36"x36"
W75 (LT.or RT.)	LANE ENDS MERGE LEFT	36"x36"
TYPE K	TYPE K MARKER (HORIZONTAL)	15"x6"
TYPE L	TYPE L MARKER (VERTICAL)	8"x24"
TYPE N	TYPE N MARKER	18"x18"
SW1	CROSS TRAFFIC DOES NOT STOP	36"x18"
SW12B	FIRE STATION	36"x36"
SW24	SCHOOL CROSSING	30"x42"
SW32	DRIFTING SAND	36"x36"
SW44	ARROW	30"x30"
SR4	SCHOOL SPEED 25 WHEN CHILDREN	24"x48"

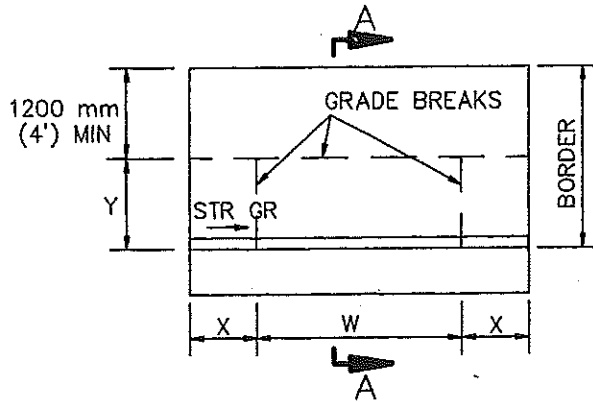
STD-PLAN
PW-13
SHT. 2 OF 2

SUBMITTED: Bob Wittgen 8/14/99
TRAFFIC ENGINEERING MANAGER DATE

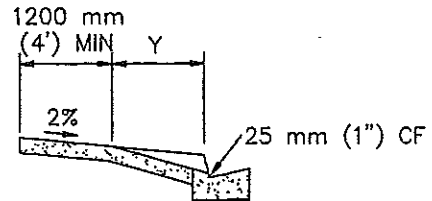
CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS

APPROVED: J. H. Long 12 Aug 99
DIRECTOR OF PUBLIC WORKS DATE

STANDARD TRAFFIC CONTROL
SIGNS



TYPE A

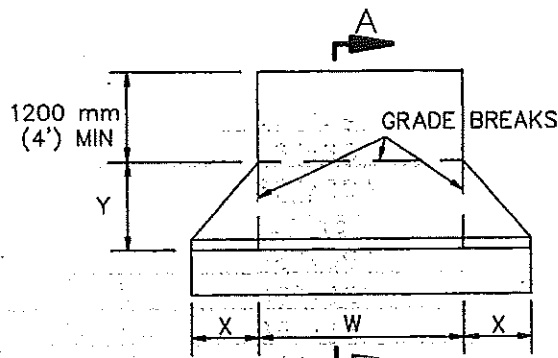


SECTION A-A

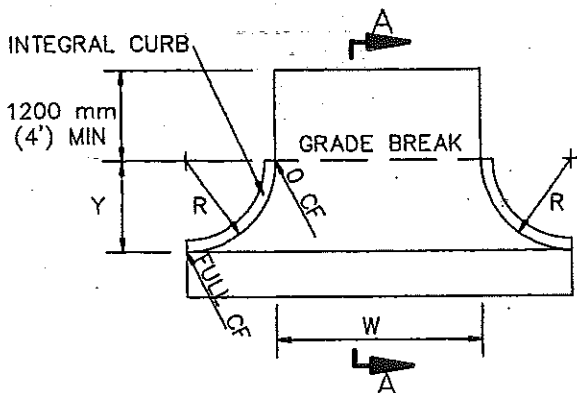
CURB FACE, mm	X, mm	Y, mm
150 (6") or less	900 (3'-0")	1200 (4'-0")
175 (7")	1050 (3'-6")	1425 (4'-9")
200 (8")	1200 (4'-0")	1700 (5'-8")
225 (9")	1350 (4'-6")	1950 (6'-6")
250 (10")	1500 (5'-0")	2175 (7'-3")
275 (11")	1650 (5'-6")	2400 (8'-0")
300 (12") or more	1800 (6'-0")	2625 (8'-9")

NOTES:

1. RESIDENTIAL DRIVEWAYS SHALL BE 100 mm (4") THICK PCC.
2. COMMERCIAL DRIVEWAYS SHALL BE 150 mm (6") THICK PCC.
3. WEAKENED PLANE JOINTS SHALL BE INSTALLED AT BOTH SIDES OF A DRIVEWAY AND AT APPROXIMATELY 3000 mm (10') INTERVALS.
4. CURB FOR TYPE C DRIVEWAY SHALL BE INTEGRAL AND MATCH ADJACENT CONSTRUCTION.
5. REFER TO LOCAL DEVELOPMENT REGULATIONS FOR AMERICANS WITH DISABILITIES ACCESS REQUIREMENTS AND MAXIMUM PERMITTED DRIVEWAY WIDTHS.



TYPE B



TYPE C

STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION

PROMULGATED BY THE
PUBLIC WORKS STANDARDS INC.
GREENBOOK COMMITTEE
1984
REV. 1988

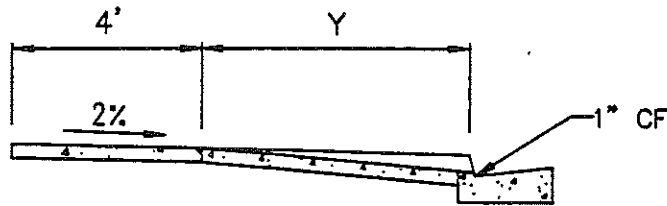
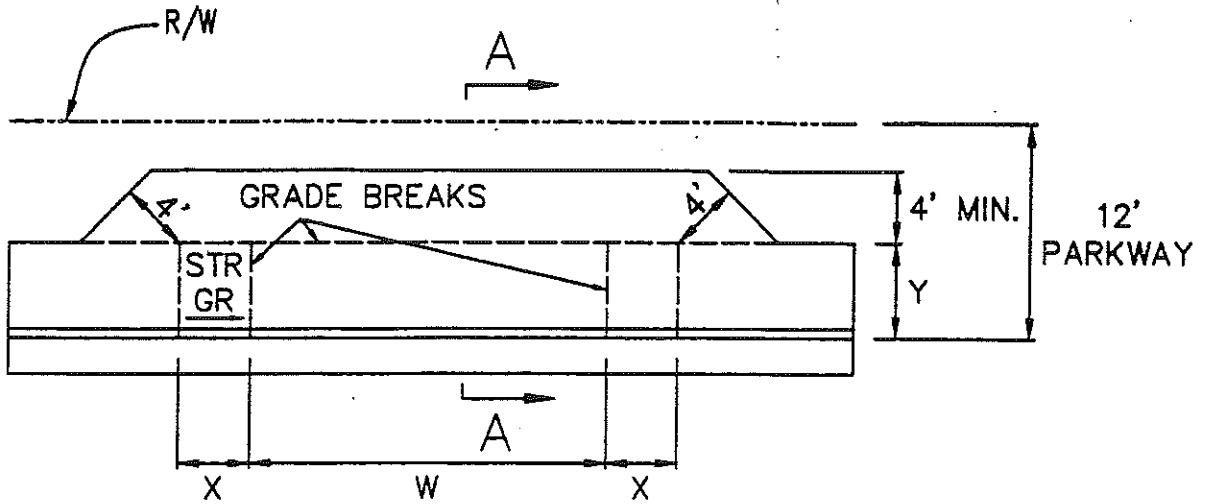
DRIVEWAY APPROACHES

USE WITH STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION

STANDARD PLAN
METRIC

110-1

SHEET 1 OF 1



SECTION A-A

CURB FACE	X	Y
6"	3'-0"	4'-0"
8"	4'-0"	5'-8"

NOTES:

1. RESIDENTIAL DRIVEWAYS SHALL BE 4" THICK PCC.
2. WEAKENED PLANE JOINTS SHALL BE INSTALLED AT BOTH SIDES OF A DRIVEWAY AND AT APPROXIMATELY 10' INTERVALS.
3. REFER TO CITY OF LANCASTER DEVELOPMENT REGULATIONS FOR MAXIMUM PERMITTED DRIVEWAY WIDTHS.

REVISED 12-18-00

CITY OF LANCASTER
DEPARTMENT OF PUBLIC WORKS

RESIDENTIAL DRIVEWAY STANDARD

SUBMITTED:

CITY ENGINEER

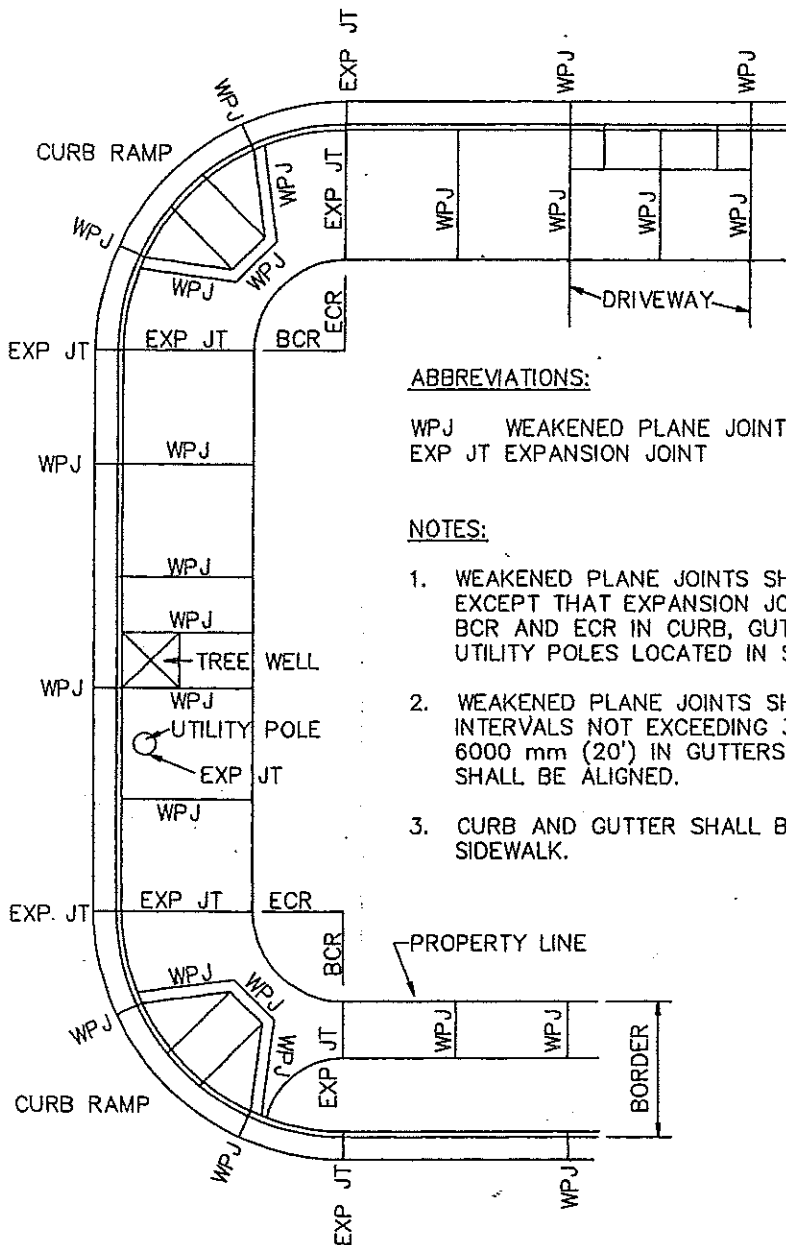
[Signature]
DIRECTOR OF PUBLIC WORKS

12-19-00
DATE

2 JAN 01
DATE

STD. PLAN

PW-14



ABBREVIATIONS:

WPJ WEAKENED PLANE JOINT BCR BEGINNING OF CURB RETURN
 EXP JT EXPANSION JOINT ECR END OF CURB RETURN

NOTES:

1. WEAKENED PLANE JOINTS SHALL BE USED FOR ALL JOINTS, EXCEPT THAT EXPANSION JOINTS SHALL BE PLACED AT THE BCR AND ECR IN CURB, GUTTER AND SIDEWALK, AND AROUND UTILITY POLES LOCATED IN SIDEWALK AREAS.
2. WEAKENED PLANE JOINTS SHALL BE CONSTRUCTED AT REGULAR INTERVALS NOT EXCEEDING 3000 mm (10') IN WALKS AND 6000 mm (20') IN GUTTERS. JOINTS IN CURB AND WALK SHALL BE ALIGNED.
3. CURB AND GUTTER SHALL BE CONSTRUCTED SEPARATELY FROM SIDEWALK.

STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION

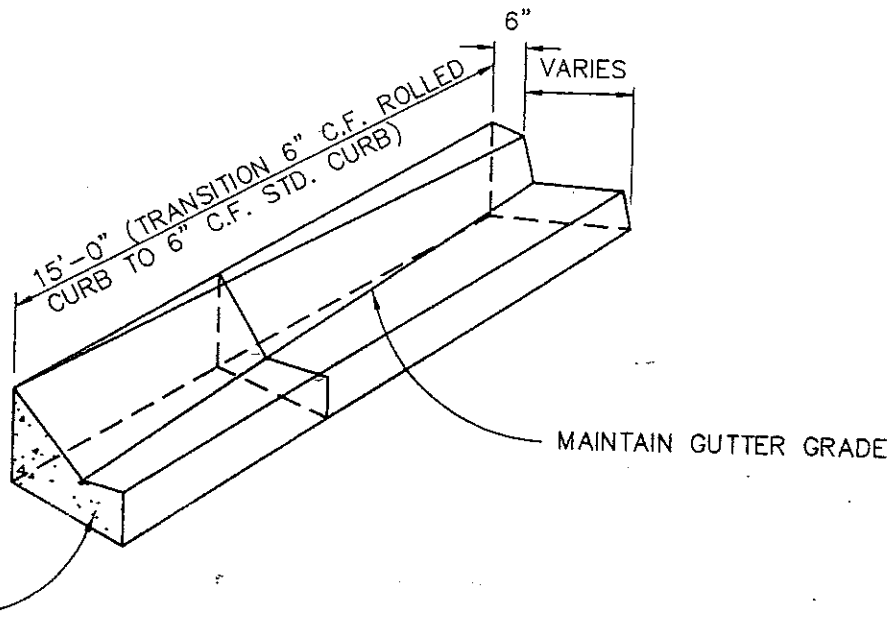
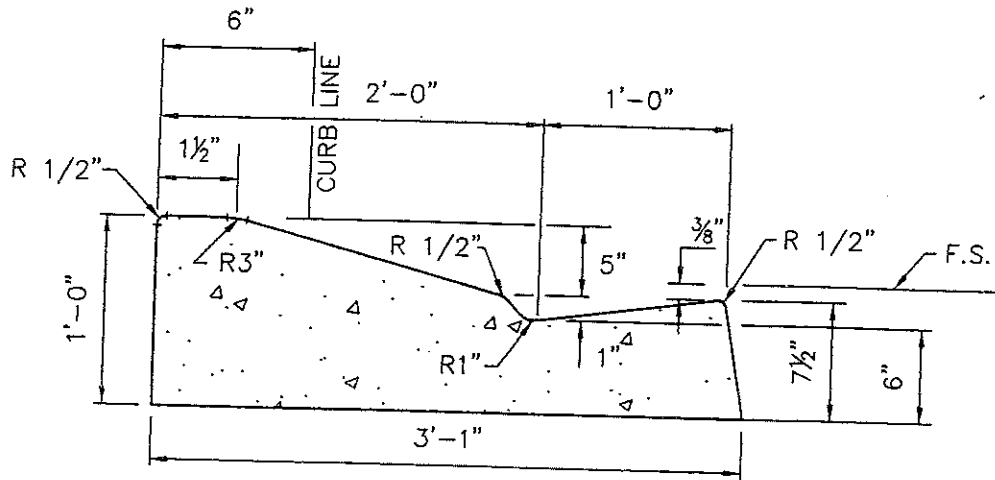
PROMULGATED BY THE
 PUBLIC WORKS STANDARDS INC.
 GREENBOOK COMMITTEE
 1984
 REV. 1998

CURB AND SIDEWALK JOINTS

USE WITH STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION

STANDARD PLAN
 METRIC
112-1

SHEET 1 OF 1



NOTE:

TRANSITION TO START NOT LESS THAN 15' BEYOND
END OF CURB RETURN.

03/05/04

STD. PLAN
PW-15

SUBMITTED:

Steve Chen
CITY ENGINEER

3/9/04
DATE

CITY OF LANCASTER

DEPARTMENT OF PUBLIC WORKS

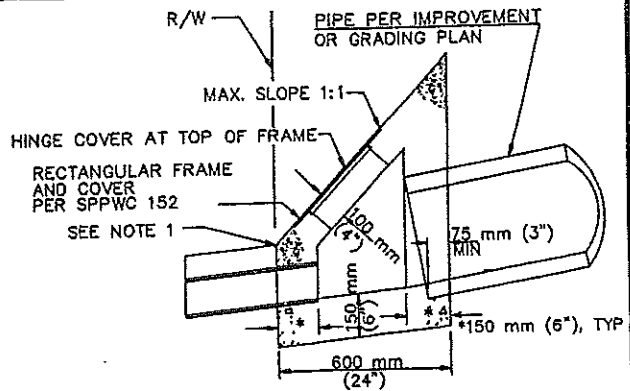
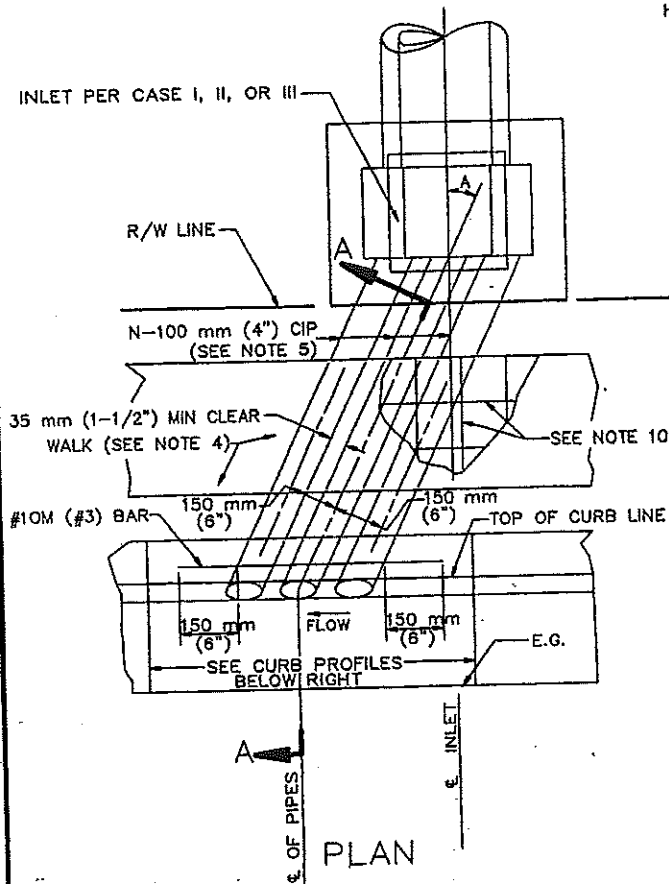
APPROVED:

James R. Williams
DIRECTOR OF PUBLIC WORKS

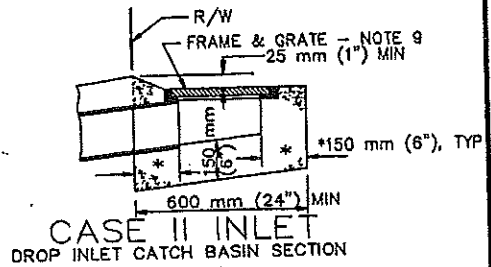
3/10/04
DATE

CONCRETE ROLLED CURB

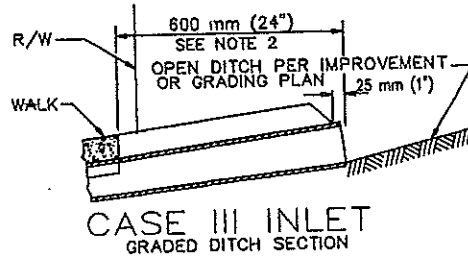
RURAL STREET



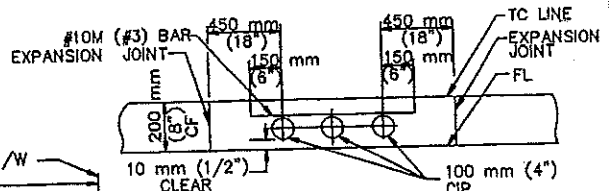
CASE I INLET
TRANSITION STRUCTURE SECTION



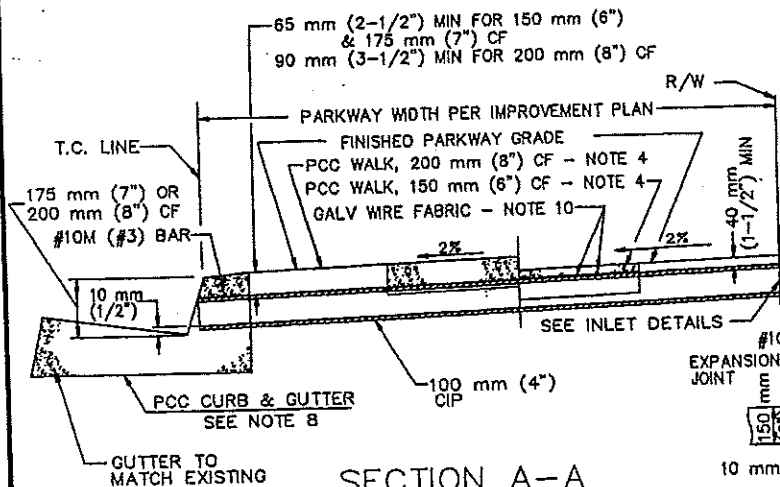
CASE II INLET
DROP INLET CATCH BASIN SECTION



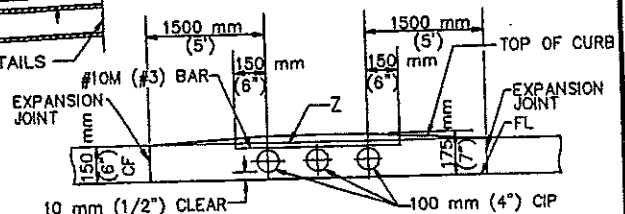
CASE III INLET
GRADED DITCH SECTION



NOTE: APPLIES TO ANY NUMBER OF PIPES
CURB PROFILE
200 mm (8") CURB FACE



SECTION A-A



NOTE: APPLIES TO ANY NUMBER OF PIPES
Z = TOP-OF-CURB LINE SHOWN ON PROFILE

CURB PROFILE
150 mm (6") CURB FACE

STANDARD PLANS FOR PUBLIC WORKS CONSTRUCTION

PROMULGATED BY THE
PUBLIC WORKS STANDARDS INC.
GREENBOOK COMMITTEE
1984
REV. 1992, 1998

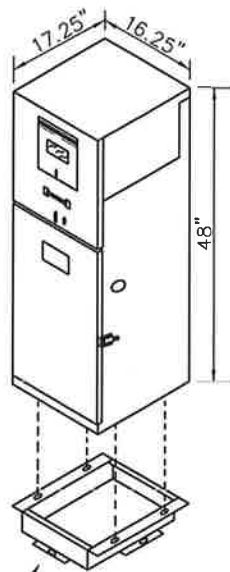
CURB DRAIN

STANDARD PLAN
METRIC

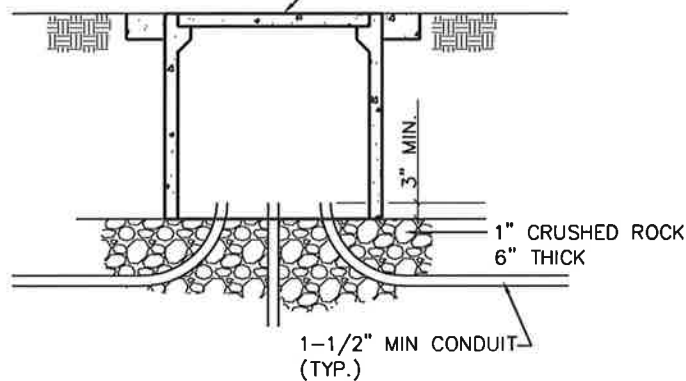
150-2

USE WITH STANDARD SPECIFICATIONS FOR PUBLIC WORKS CONSTRUCTION

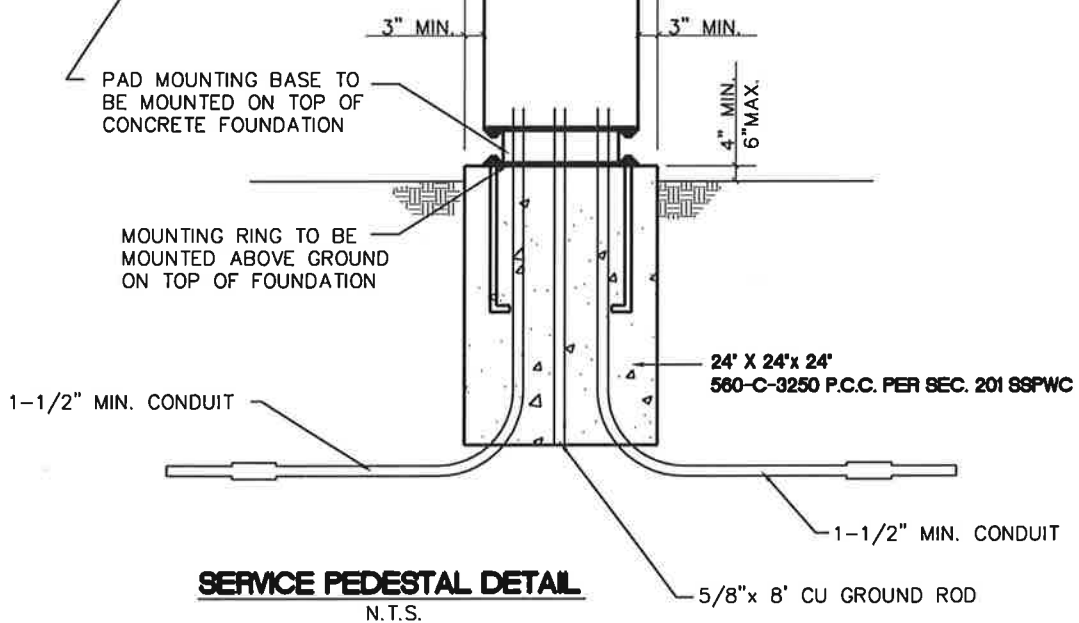
SHEET 1 OF 2



BOLT DOWN COVER TO BE INSCRIBED WITH "STREET LIGHT"




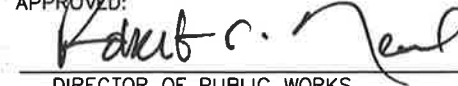
PULLBOX DETAIL
N.T.S.



SERVICE PEDESTAL DETAIL
N.T.S.

NOTES:

1. STANDARD VOLTAGE SHALL BE SUITABLE FOR USE ON 120/240V SERVICE. COORDINATE WITH SCE TO VERIFY THE AVAILABLE SERVICES AND POINT OF CONNECTION.
2. SERVICE PEDESTAL SHALL BE MYERS ELECTRIC PRODUCTS MEUG16 WITH STANDARD FEATURES AND PER PRODUCT SPECIFICATIONS OR APPROVED EQUAL. COLOR SHALL BE WHITE ONLY.
3. PAD MOUNTING BASE FOR CONCRETE FOUNDATION SHALL BE MYERS ELECTRIC PRODUCTS MEUG16-BASE OR APPROVED EQUAL.
4. PULLBOX SHALL BE BROOKS TYPE NO 3-1/2 PCC OR APPROVED EQUAL PER SSPWC STD. 405-1.
5. PULLBOX SHALL BE INSTALLED WITHIN 5' OF PEDESTAL WITH 1-1/2" MIN. CONDUIT
6. CONDUITS SHALL EXTEND INTO CABINET 1" MIN. ABOVE FLOOR AND HAVE APPROVED CONDUIT BUSHING.
7. ANCHOR BOLT INSTALLATION FOR MOUNTING BASE, USE 1/2"-13 BOLTS OR 5/8"-18 BOLTS.
8. LOCATION OF SERVICE PEDESTAL SHALL BE PER CITY OF LANCASTER STREET LIGHT GENERAL NOTES AND SPECIFICATIONS.

SUBMITTED:  CITY ENGINEER	DEPARTMENT OF PUBLIC WORKS STANDARD PLAN
APPROVED:  DIRECTOR OF PUBLIC WORKS	Service Pedestal And Pullbox Detail PW-20

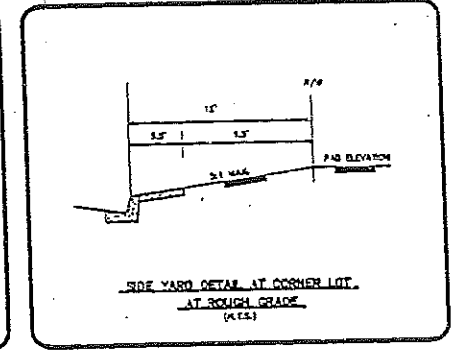
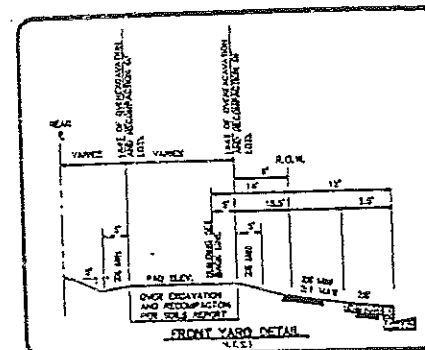
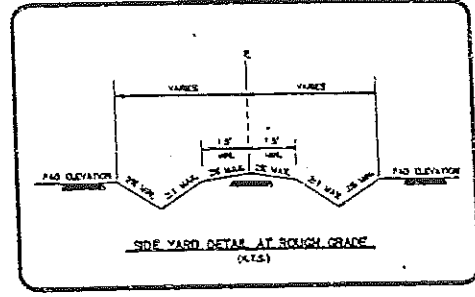
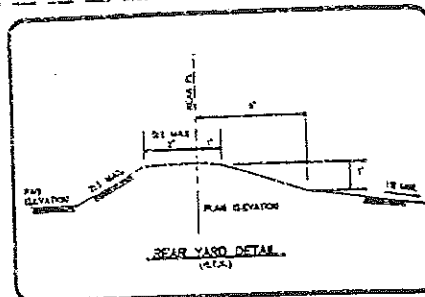
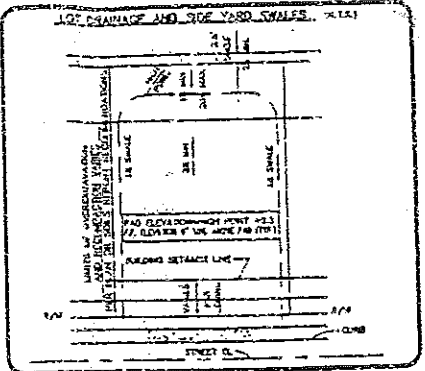
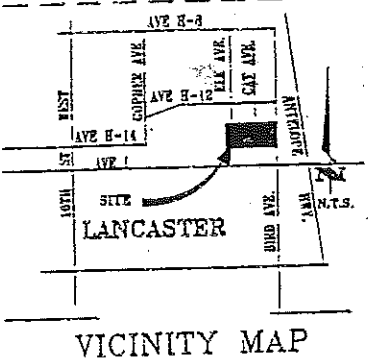
7-19-10
DATE

8/11/10
DATE

ROUGH GRADING PLANS

TRACT 00000 (36" MAX.)

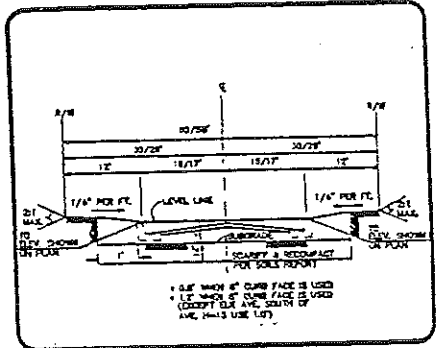
SEE CURRENT GRADING GENERAL NOTES IN THE CITY GRADING PLAN SUBMITTAL REQUIREMENTS PACKET



THIS EARTHWORK QUANTITIES CHART SHALL SERVE AS A GUIDE FOR FORMAT. EARTHWORK-USE SCENARIOS MAY VARY PER PROJECT. (CHART SHALL BE MUCH SMALLER SCALE THAN DEPICTED)

ENGINEER'S ESTIMATE OF EARTHWORK QUANTITIES (FOR PERMIT PURPOSES ONLY)			
DESCRIPTION	CUT (CUBIC YARDS)	FILL (CUBIC YARDS)	
LOT(S) (RAW)	"C1" (AMOUNT OF C.Y. OF RAW CUT)	"F1" (AMOUNT OF C.Y. OF RAW FILL)	
SCARIFICATION (AFFECTED BUILDING AREA & PARKING LOT CUBIC YARDS - "A" SCARIFICATION SHRINKAGE ("S1") (MAXIMUM OF 2 RANGE GIVEN BY SOILS ENGINEER IN THE SOILS REPORT)		"S1" = ((F1) x (MAX. % SHRINKAGE) + (A) x (MAX. % SHRINKAGE))	
SCARIFICATION SUBSIDENCE ("S2") (MAXIMUM OF 2 RANGE GIVEN BY SOILS ENGINEER IN THE SOILS REPORT)		"S2" = (MAX. DEPTH OF A FOOT SUBSIDENCE x (AREA OF TOTAL GRADING IMPROVEMENTS IN SQUARE FEET)) / 27	
OVEREXCAVATION (BUILDING)	"C2" (AMOUNT OF C.Y. OF BUILDING OVEREXCAVATION CUT BEYOND RAW CUT)	"F2" (AMOUNT OF C.Y. OF BUILDING OVEREXCAVATION FILL BEYOND RAW FILL TO BUILD UP PAD)	NOTE: "C2" = "F2"
OVEREXCAVATION SHRINKAGE ("S3") (MAXIMUM OF 2 RANGE GIVEN BY SOILS ENGINEER IN THE SOILS REPORT)		"S3" = ((F2) x (MAX. % SHRINKAGE))	
OVEREXCAVATION (PARKING LOT)	"C3" (AMOUNT OF C.Y. OF PARKING LOT OVEREXCAVATION CUT BEYOND RAW CUT)	"F3" (AMOUNT OF C.Y. OF PARKING LOT OVEREXCAVATION FILL BEYOND RAW FILL)	NOTE: "C3" = "F3"
OVEREXCAVATION SHRINKAGE ("S4") (MAXIMUM OF 2 RANGE GIVEN BY SOILS ENGINEER IN THE SOILS REPORT)		"S4" = ((F3) x (MAX. % SHRINKAGE))	
BASIN	"C4" (AMOUNT OF C.Y. OF BASIN CUT BEYOND RAW CUT)		
TOTALS	TOTAL CUT = "C1" + "C2" + "C3" + "C4"	TOTAL FILL = ("F1" + "F2" + "F3") + ("S1" + "S2" + "S3" + "S4")	NOTE: TAKE THE GREATER OF TOTAL CUT OR TOTAL FILL FOR QUANTITY OF EARTHWORK IN FINISHING GRADING BOND ESTIMATE
	IF (TOTAL CUT > TOTAL FILL), THEN TOT. CUT - TOT. FILL = EXPORT (IN C.Y.)	IF (TOTAL FILL > TOTAL CUT), THEN TOT. FILL - TOT. CUT = (IMPORT) X 1.3 (IN C.Y.)	

- CONSTRUCTION NOTES:**
- CONSTRUCT ... PER A.P.W.A. STD. PLAN 000-0 ...
 - CONSTRUCT ... PER A.P.W.A. STD. PLAN 000-0 ...
 - REMOVE EXISTING ... AND PROPERLY DISPOSE ...
 - INSTALL 00 L.F. OF 00" RCP ...
 - CONSTRUCT ... PER A.P.W.A. STD. PLAN 000-0 ...
 - REMOVE AND REPLACE EXISTING ... WITH ...
 - CONSTRUCT ... PER A.P.W.A. STD. PLAN 000-0 ...



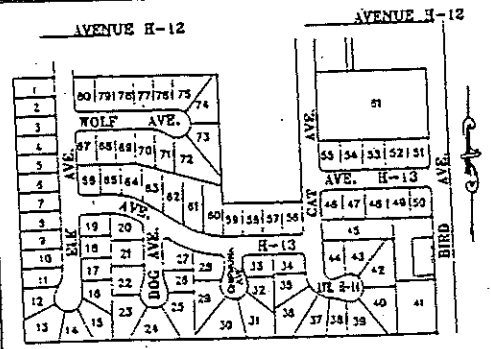
BENCH MARK CL 0000
 ELEVATION: 2224.957
 CORNER OF SW COR
 AVE. H-8 & BIRD AVE. CL
 19 FT S & 20 FT W C/L INT.
 LANCASTER 0240 (1991)
 (VERBATIM)

LEGAL DESCRIPTION:
 TRACT NO. 00000 IN THE CITY OF LANCASTER, COUNTY OF LOS ANGELES, STATE OF CALIFORNIA BEING A SUBDIVISION OF A PORTION OF THE SE 1/4 OF THE SW 1/4 OF SEC. 10, T.7 N., R. 12 W., S.B.M. (81 LOTS, 21.42 ACRES)

ENGINEER'S NOTICE TO CONTRACTOR:
 THE EXISTENCE AND LOCATION OF ANY AND ALL EXISTING UTILITY PIPES AND STRUCTURES SHOWN ON THIS SET OF PLANS ARE OBTAINED BY A SEARCH OF THE AVAILABLE RECORDS AT THE TIME OF DESIGN. THE CONTRACTOR IS RESPONSIBLE FOR VERIFYING THE LOCATION AND DEPTH OF ALL UTILITIES PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CITY OF LANCASTER AND THE CALIFORNIA DEPARTMENT OF WATER RESOURCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CALIFORNIA DEPARTMENT OF WATER RESOURCES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS AND APPROVALS FROM THE CALIFORNIA DEPARTMENT OF WATER RESOURCES.

SPECIAL NOTES:
 1. CONTRACTOR IS TO FAMILIARIZE HIMSELF WITH THE OBSTRUCTION ON EACH LOT ABUTTING THE TRACT BOUNDARY AND DETERMINE THE EXTENT OF REMOVAL OF CONCRETE BLOCK WALLS, ETC. TO ALLOW FOR CONSTRUCTION OF NEW WALL INCLUDING OVEREXCAVATION AND RECOMPACTION PER THE SOILS REPORT.

- LEGEND:**
- DRAINAGE SWALE OR DIRECTION OF FLOW
 - SLOPE BANK (2:1) UNLESS SHOWN OTHERWISE
 - EXISTING CONTOUR ELEVATION FROM AERIAL PHOTO (19)
 - DAYLIGHT LINE
 - FINISH GRADE ELEVATION
 - PAD ELEVATION
 - PROPOSED BLOCK WALL / RETAINING WALL
 - FS FINISHED SURFACE
 - FG FINISHED GRADE



ALTERNATE LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYP. EACH SHEET)

RECORD DRAWING:
 THESE "RECORD" DRAWINGS WERE PREPARED FROM INFORMATION FURNISHED ENTIRELY OR IN PART BY OTHERS. NO WARRANTY, EXPRESSED OR IMPLIED IS MADE OR INTENDED BY THE ENGINEER OF RECORD AS TO COMPLETENESS OR ACCURACY OF DATA INCORPORATED INTO THESE DRAWINGS AS A RESULT OF WORK BY OTHERS.

SOILS ENGINEER:
 THESE PLANS HAVE BEEN REVIEWED AND APPEAR TO BE IN GENERAL CONFORMANCE WITH THE RECOMMENDATIONS OF OUR REPORT DATED 05/02/00 FILE NO. 000000. WE MAKE NO REPRESENTATIONS AS TO DIMENSIONS, MEASUREMENTS, CALCULATIONS OR ANY PORTION OF THE DESIGN.
 PROJECT ENGINEER: (NAME) CALIF. REG. NO. _____
 (SIGNATURE) DATE: _____



PLANS PREPARED UNDER THE SUPERVISION OF JOHN G. DOE FOR
 (CLIENT NAME AND COMPANY)
COMPANY LOGO
 (SIGNATURE) DATE: _____



RECORD NO.	REVISION DESCRIPTION	DATE

CITY OF LANCASTER	
RECORD ENGINEER	DATE
CITY STAFF	DATE
RECORD DRAWING	DATE

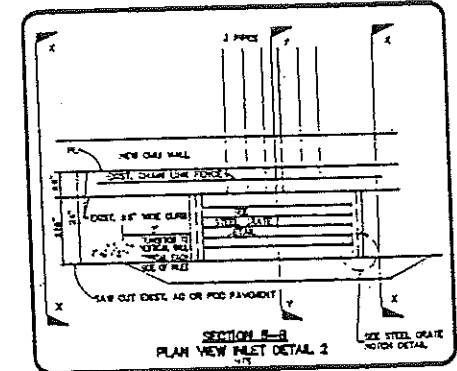
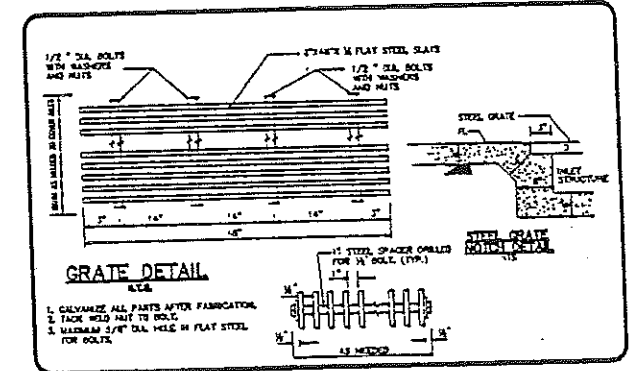
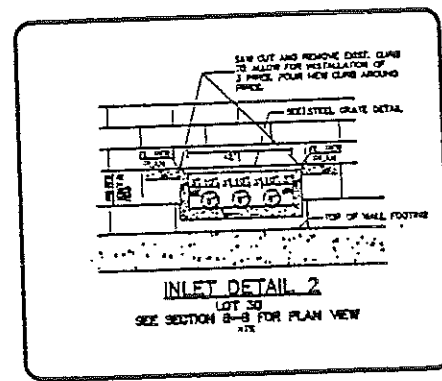
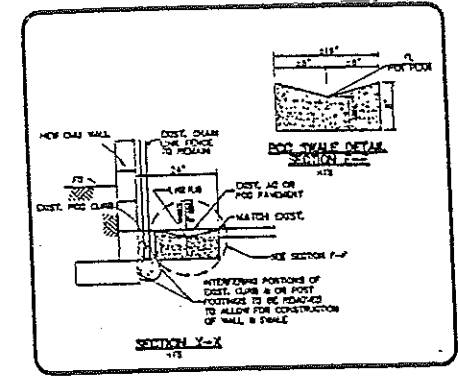
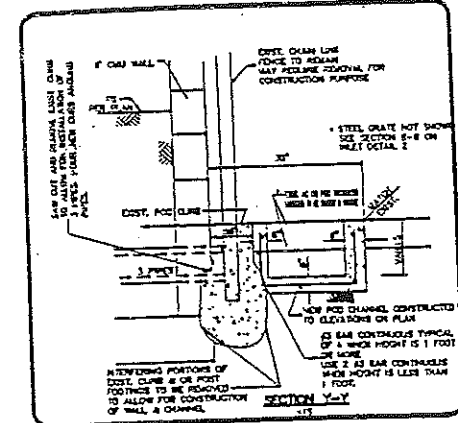
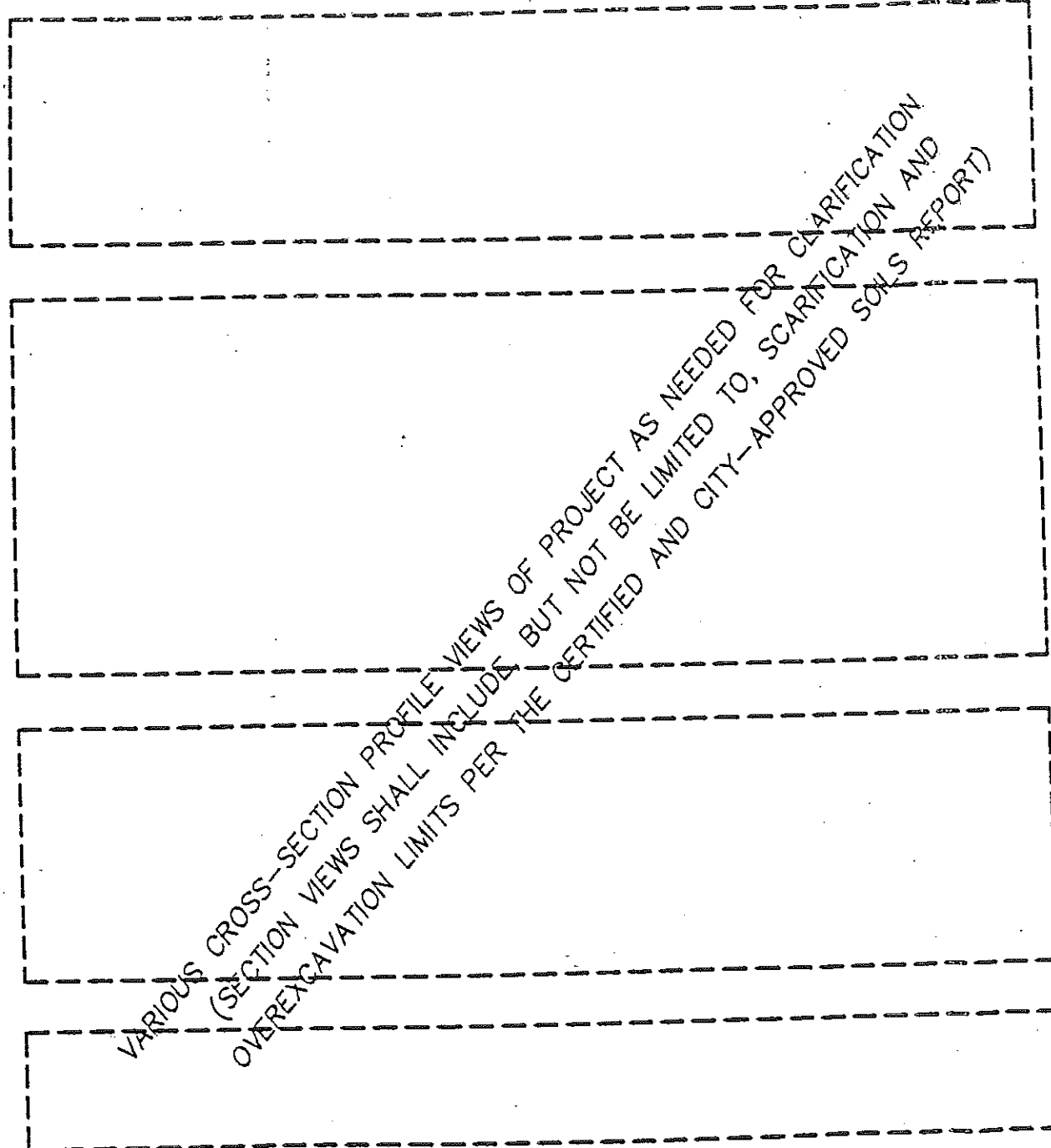
CITY OF LANCASTER
 ROUGH GRADING PLAN
 TR 00000
 COVER SHEET
 SCALE: NONE

SHEET
 1
 OF
 3
 SHTS

PMT. 00-0000

(36" MAX.)

(24" MAX.)



ALTERNATE LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYP. EACH SHEET)

RECORD DRAWING
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PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. DOE FOR (CLIENT NAME AND COMPANY).
COMPANY LOGO
 (SIGNATURE) _____ DATE _____



REVISION	DATE	DESCRIPTION	BY	DATE
1				

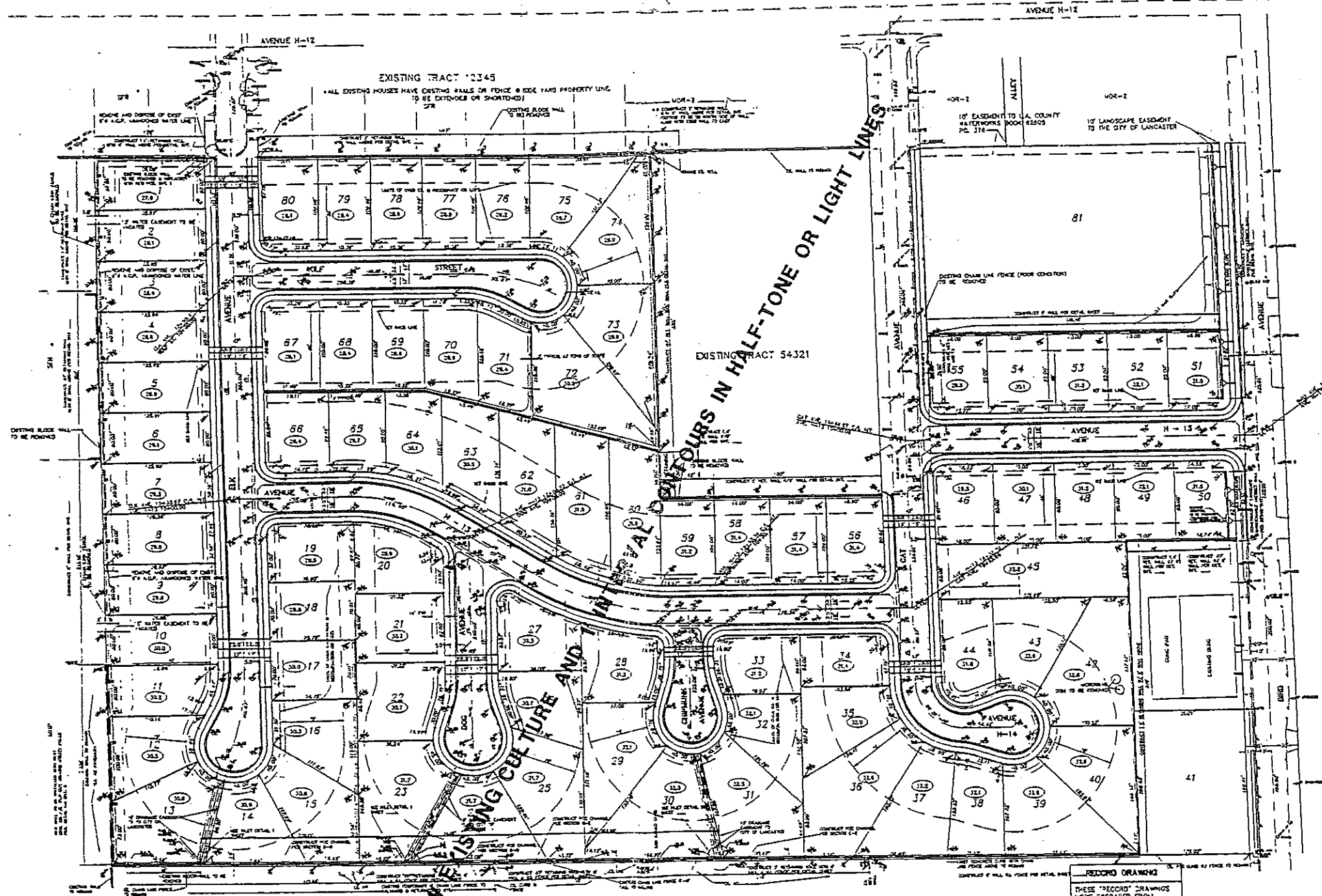
APPROVED BY	DATE
PLAN CHECK ENGINEER	
CITY ENGINEER	
DIRECTOR OF PUBLIC WORKS	

CITY OF LANCASTER
 ROUGH GRADING PLAN
 TR 00000
 DETAILS AND SECTIONS

SHEET
 2 OF 3
 SHTS

(36" MAX.)

(24" MAX.)



SCALE 1"=40'

PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. JOE FOR
 CLIENT NAME AND COMPANY
COMPANY LOGO
 (SIGNATURE) DATE



REVISION	REVISION DESCRIPTION	DATE

ALTERNATE LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYP. EACH SHEET)
CITY OF LANCASTER

REVIEWED BY	DATE
CITY ENGINEER	
APPROVED BY	DATE
DIRECTOR OF PUBLIC WORKS	

RECORD DRAWINGS
 THESE "RECORD" DRAWINGS WERE PREPARED FROM INFORMATION FURNISHED DIRECTLY OR IN PART BY OTHERS. NO WARRANTY, EXPRESSED OR IMPLIED IS MADE OR INTENDED BY THE ENGINEER OF RECORD AS TO COMPLETENESS OR ACCURACY OF DATA INCORPORATED INTO THESE DRAWINGS AS A RESULT OF WORK BY OTHERS.

CITY OF LANCASTER
 ROUGH GRADING PLAN
 TR 00000

SHEET
 3
 3

PMT. 00-0000

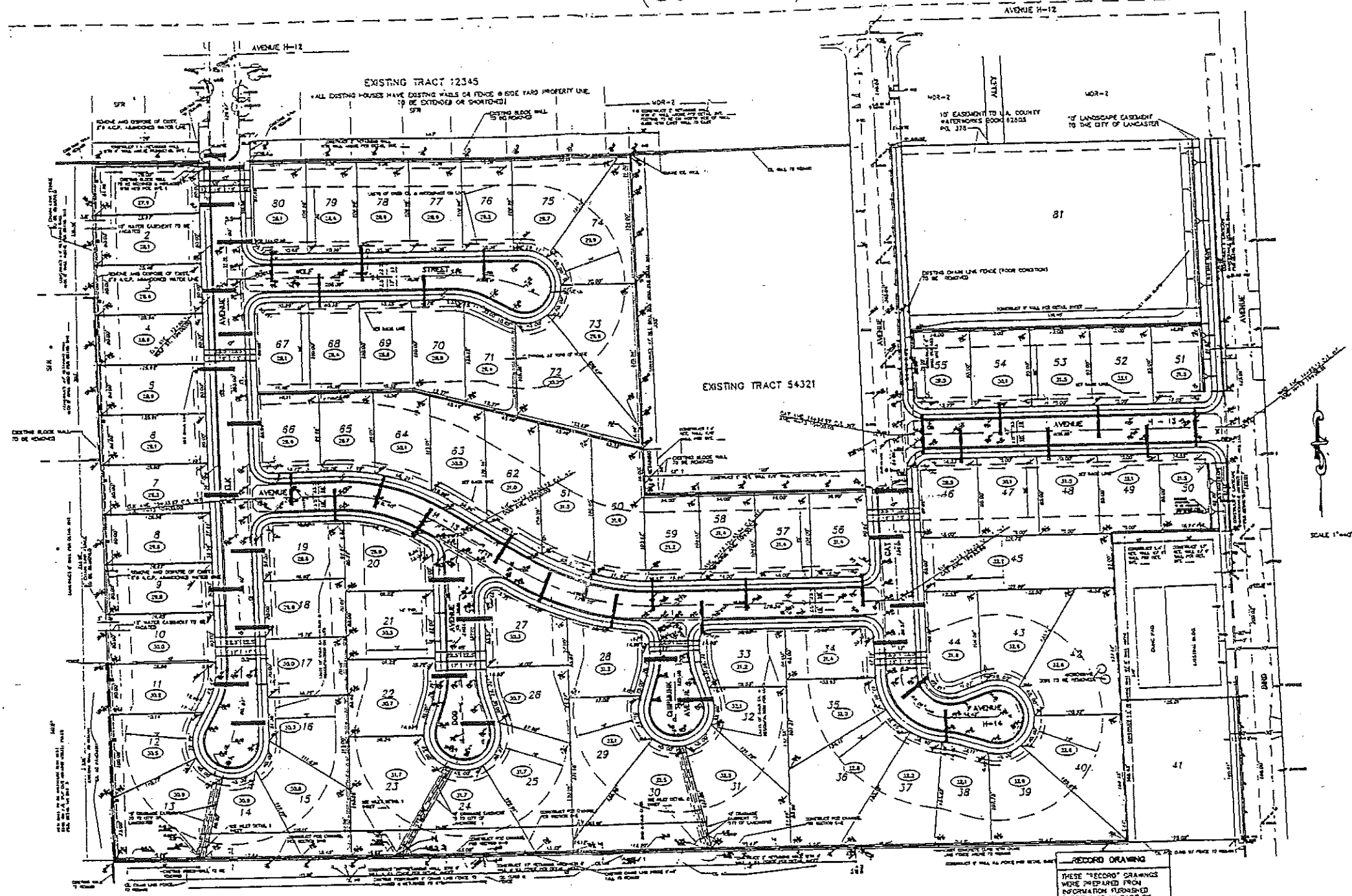
SEE CURRENT EROSION CONTROL NOTES AND DETAILS IN THE CITY GRADING PLAN SUBMITTAL REQUIREMENTS PACKET.

(36" MAX.)

(24" MAX.)

CHECKED BY PLAN CHECK ENGINEER	DATE	REVIEWED BY CITY ENGINEER	DATE	APPROVED BY DIRECTOR OF PUBLIC WORKS	DATE
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PREFERRED LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYPICAL EVERY SHEET)



PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. DOE FOR (CLIENT NAME AND COMPANY)

COMPANY LOGO

SIGNATURE: _____ DATE: _____



REVISION BLOCK	REVISION DESCRIPTION	DATE

ALTERNATE LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYP. EACH SHEET)

CITY OF LANCASTER

CHECKED BY: PLAN CHECK ENGINEER DATE: _____

REVIEWED BY: CITY ENGINEER DATE: _____

APPROVED BY: DIRECTOR OF PUBLIC WORKS DATE: _____

RECORD DRAWING

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RECORD ENGINEER DATE: _____

CITY STAFF DATE: _____

RECORD DRAWING DATE: _____

CITY OF LANCASTER
ROUGH GRADING PLAN
TR 00000
EROSION CONTROL PLAN

SHEET 1 OF 1

PMT. 00-0000

EROSION CONTROL PLAN (SAMPLE) - PLATE 10.1.4

(36" MAX.)

DESIGNED BY PLAN CHECK ENGINEER	DATE	REVIEWED BY CITY ENGINEER	DATE	APPROVED BY DIRECTOR OF PUBLIC WORKS	DATE
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PREFERRED LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYPICAL EVERY SHEET)

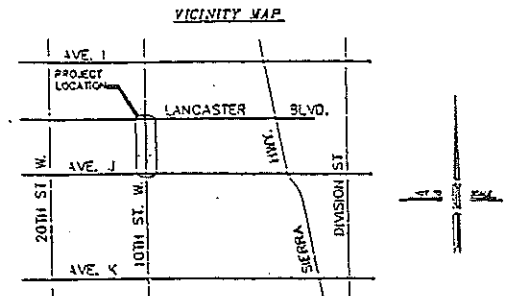
CONSTRUCTION NOTES:

- CONSTRUCT P.C.C. CURB AND GUTTER TYPE A1-B PER A.P.W.A. STANDARD PLAN 120-0.
- CONSTRUCT 2" THICK A.C. PAVEMENT SURFACE COURSE, TYPE CE-184000 ON 4" THICK A.C. PAVEMENT BASE COURSE, TYPE B-184000 OR 6" CRUSHED AGGREGATE BASE.
- REMOVE EXISTING A.C. PAVEMENT AND AGGREGATE BASE. RECONSTRUCT 2" THICK A.C. PAVEMENT SURFACE COURSE, TYPE CE-184000 ON 4" THICK A.C. PAVEMENT BASE COURSE, TYPE B-184000 OR 6" CRUSHED AGGREGATE BASE.
- CONSTRUCT 4" THICK P.C.C. SIDEWALK PER A.P.W.A. STANDARD PLAN NO. 111-0.
- COLD FRAME EXISTING PAVEMENT TO A DEPTH OF 2". CONSTRUCT 2" THICK A.C. PAVEMENT OVERLAY, TYPE CE-184000 MONOLITHIC WITH 2" THICK SURFACE COURSE.
- REMOVE EXISTING CHAIN LINK FENCE, CONSTRUCT NEW CHAIN LINK FENCE AT THE RIGHT OF WAY LINE, PROPERLY DISPOSED OF FENCE MATERIALS PERMITTED.
- CONSTRUCT 2" THICK A.C. PAVEMENT SURFACE COURSE, TYPE CE-184000 ON 4" THICK A.C. PAVEMENT BASE COURSE, TYPE B-184000 OR 6" CRUSHED AGGREGATE BASE WITH TENSAR GEOTEXTILE, TYPE SIZ. AT NO-LAYER AND SUBGRADE-BASE INTERFACE.
- REMOVE EXISTING A.C. PAVEMENT AND AGGREGATE BASE. RECONSTRUCT 2" THICK A.C. PAVEMENT SURFACE COURSE, TYPE CE-184000 ON 4" THICK A.C. PAVEMENT BASE COURSE, TYPE B-184000 OR 6" CRUSHED AGGREGATE BASE WITH TENSAR GEOTEXTILE, TYPE SIZ. AT NO-LAYER AND SUBGRADE-BASE INTERFACE.
- EXISTING FIRE HYDRANT TO BE RELOCATED BY OTHERS.
- REMOVE EXISTING TRAFFIC SIGNAL PULL BOX, INSTALL A NUMBER 1/8" PULL BOX WITH STEEL OUT BOXING PROPOSED CURB FACE. REPLACE EXISTING VEHICLE DETECTOR LOOP AND INSTALL ADDITIONAL VEHICLE DETECTOR LOOPS AS DIRECTED BY THE ENGINEER.
- CONSTRUCT CURB RAMP PER A.P.W.A. STANDARD PLAN 111-0, CASE E.
- EXISTING POWER POLE TO BE REMOVED OR RELOCATED BY OTHERS.
- REMOVE EXISTING VEHICLE DETECTOR HANGHOLE, INSTALL NEW VEHICLE DETECTOR HANGHOLE AS DIRECTED BY THE ENGINEER.
- EXISTING GAS SERVICE VALVE TO BE RELOCATED BY OTHERS.
- INSTALL SPECIAL BUS SHELTER, 20' x 12' x 4' ALUMINUM MODEL NO. 8025-2335L-11 INCLUDING ALUMINUM BENCH WITH 48" HANDICAP ACCESS AND A 20 WATT SOLAR PANEL BY ACE ALUMINUM COMPANY OF AN APPROVED EQUAL. STATION SHOWN IS CENTERLINE OF PROPOSED BUS SHELTER.
- ADJUST VEHICLE FRAME AND COVER TO GRADE.
- REMOVE EXISTING P.C.C. CURB, GUTTER, CROSS-GUTTER, SIDEWALK, DRIVEWAYS, LANDSCAPE, ETC. AND DISPOSE OF PROPERLY.
- REMOVE TREE AND / OR STUMP, ORING STUMP 24-INCHES BELOW FINISHED SURFACE.
- EXISTING TRAFFIC SIGN AND POST TO BE RELOCATED BY OTHERS.
- REMOVE EXISTING 3 LAYER BRICK WALLS FROM RIGHT-OF-WAY. RECONSTRUCT 3 LAYER BRICK WALLS BOUND THE RIGHT-OF-WAY LINE. USE THE SAME MATERIALS, OR AN APPROVED EQUAL, WITH AN EXISTING BRICK. BRICKS THAT HAVE NOT BEEN DAMAGED BY THE CITY'S MAINTENANCE YARD TO SCHEDULE DELIVERY OF THE SALVAGED BRICKS. EXISTING PLANTS, TRANSPLANT IN CONTAINERS, AND STORE ON THE OWNER'S PROPERTY.
- CONSTRUCT 6" P.C.C. DRIVEWAY APPROACH PER A.P.W.A. STANDARD PLAN 110-0 (TYPE A1).
- RELOCATE EXISTING FLAG POLE HALFWAY BETWEEN RIGHT-OF-WAY LINE AND THE EXISTING BUILDING AT THE SAME STATION.
- EXISTING SIGN AND POST TO BE REMOVED OR RELOCATED BY OTHERS.
- REMOVE EXISTING STAMP, STAMP, GUTTER AND CROSS GUTTER AND DISPOSE OF PROPERLY. CONSTRUCT A.C. PAVEMENT AND BASE PER NOTE NO. 2. CONSTRUCT NEW P.C.C. CURB AND GUTTER PER NOTE NO. 1.
- DEMOLISH AND PROPERLY DISPOSE OF EXISTING STAIRS. CONSTRUCT NEW P.C.C. STAIRS TO MATCH EXISTING WALKWAYS.
- REMOVE EXISTING TRAFFIC SIGNAL PULL BOX AND DISPOSE OF PROPERLY.
- CONSTRUCT A BLOCK RETAINING WALL PER DETAIL W-1 FOUND IN THE APPROX.
- CONSTRUCT A BLOCK RETAINING WALL PER DETAIL W-2 FOUND IN THE APPROX.
- DEMOLISH AND PROPERLY DISPOSE OF EXISTING P.C.C. DRIVEWAY.
- REMOVE PORTION OF EXISTING BLOCK WALL TO AT 6/0 C/L. REMOVE ANY MORTAR FROM THE SALVAGED BLOCKS. CALL THE CITY'S MAINTENANCE YARD TO SCHEDULE DELIVERY OF THE SALVAGED BLOCKS.
- REMOVE EXISTING P.C.C. CURB AND DISPOSE OF PROPERLY. CONSTRUCT CURB TO THE PROPOSED BACK OF SIDEWALK. JOIN EXISTING CURB USING 18" LONG 1/4" REBAR COVOLS. NEW CURB SHALL BE THE SAME TYPE AND FINISH AS EXISTING CURB.
- REMOVE BRICK PILLAR AND BLOCK WALL UP TO FRONT-OF-WAY LINE. REMOVE BRICK PILLAR BEHIND FRONT-OF-WAY LINE. TIEBACK END OF BLOCK WALL TO MATCH EXISTING CONSTRUCTION FINISH. REMOVE ANY MORTAR FROM THE SALVAGED BLOCKS THAT ARE POSSIBLE. CALL THE CITY'S MAINTENANCE YARD TO SCHEDULE DELIVERY OF THE SALVAGED BLOCKS.
- RELOCATE THE EXISTING CHAIN LINK FENCE ON THE NORTHERLY PORTION OF THE PROPERTY BACK 32 FEET FROM THE CENTER LINE OF 10TH STREET WEST. REPLACE 1 3/8" PINE POSTS 32 FEET FROM CENTER LINE WITH AN EXISTING PINE POST. REMOVE EXISTING CHAIN LINK FENCE FROM THE WEST AND SOUTH SIDES OF THE PROPERTY. CLEAN CONCRETE OFF REMAINING POSTS AND PLACE THE REMAINING POSTS AND FENCE TANGS ON THE OWNER'S PROPERTY. CONTRACTOR SHALL REPAIR ALL AREAS SCRATCHED DUE TO CONSTRUCTION USING THE SAME MATERIAL AND COLOR.
- FILL AREA BY CONSTRUCTING 4" P.C.C. SIDEWALK PER A.P.W.A. STANDARD PLAN NO. 112-0.
- RELOCATE SIGN TO STATION 40+17.15, OFFSET 33 FEET FROM THE CENTERLINE OF 10TH STREET WEST. THE UNPAVED GROUND CLEARANCE SHALL BE 7 FEET. INSTEAD SIGN TO BE PERPENDICULAR TO THE CENTER LINE OF 10TH STREET WEST. PLACE SIGN POST 15 FEET AWAY FROM BLOCK WALL MEASURING FROM THE CENTER OF THE SIGN POST.
- REMOVE EXISTING SIGN AND STORE ON THE OWNER'S PROPERTY AT A LOCATION SPECIFIED BY THE OWNER.
- CONSTRUCT 4" P.C.C. ALLEY INTERSECTION APPROACH PER A.P.W.A. STANDARD PLAN 130-0 (TYPE A1). STATION SHOWN IS CENTER LINE OF PROPOSED ALLEY APPROACH.
- ASPHALT TRAFFIC SIGNAL INTERCONNECT PULL BOX.
- REMOVE EXISTING PORTIONS OF BLOCK WALL. RECONSTRUCT BLOCK WALL TO MATCH EXISTING BLOCK WALL IN THE LOCATION SHOWN ON THE PLANS. REMOVE ANY MORTAR FROM THE SALVAGED BLOCKS. CALL THE CITY'S MAINTENANCE YARD TO SCHEDULE DELIVERY OF THE SALVAGED BLOCKS.
- CONSTRUCT A PARKWAY DRAIN PER L.A.C.D.P.W. STANDARD PLAN 308-0 (INLET TYPE 21, S = 5). STATION SHOWN IS C/L OF PARKWAY DRAIN.
- CONSTRUCT A PARKWAY DRAIN PER L.A.C.D.P.W. STANDARD PLAN 308-0 (INLET TYPE 21, S = 5). STATION SHOWN IS C/L OF PARKWAY DRAIN.
- INSTALL TYPE 4 COMMUNICATION CABINET, OFFSET THE CABINET FROM THE CENTER LINE OF 10TH STREET WEST AS DIRECTED BY ENGINEER.
- REMOVE EXISTING VEHICLE DETECTOR LOOPS. INSTALL VEHICLE DETECTOR LOOPS FOR PLANS AND SPECIFICATIONS, AND AS DIRECTED BY THE ENGINEER.
- REMOVE 1" TRAFFIC SIGNAL INTERCONNECT CABLE FROM EXISTING CONDUIT BETWEEN THE PULL BOX AT APPROXIMATELY STATION 44+38 AND STATION 31+30 AND PROTECT IT ACCORDANCE WITH THE SPECIFICATIONS. ASHROCK CONDUIT BETWEEN STATIONS 44+38 AND 31+30.
- INSTALL NEW 2" SCHEDULE 40 PIP CONDUIT FROM EXISTING PULL BOX AT STATION 44+38 TO THE PROPOSED TYPE 4 COMMUNICATION CABINET AT STATION 31+30. CONDUIT SHALL BE LOCATED AS SHOWN ON THE PLANS AND INSTALLED A MINIMUM OF 18 INCHES DEEP. REINSTALL INTERCONNECT CABLE AND SPACE AT TYPE 4 COMMUNICATION CABINET IN ACCORDANCE WITH THE SPECIFICATIONS.
- CONSTRUCT CONCRETE SLOUGH WALL PER L.A.C.D.P.W. STANDARD PLAN 44-05, CASE 1, VARIABLE HEIGHT.

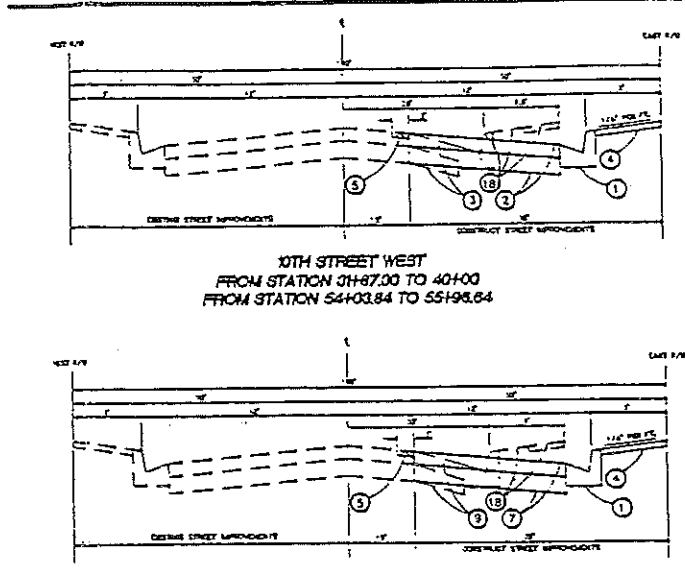
GENERAL NOTES

SEE CURRENT GENERAL NOTES IN THE CITY STREET IMPROVEMENT PLAN SUBMITTAL REQUIREMENTS PACKET

CITY OF LANCASTER
10TH STREET WEST WIDENING
BETWEEN AVENUE J AND LANCASTER BOULEVARD



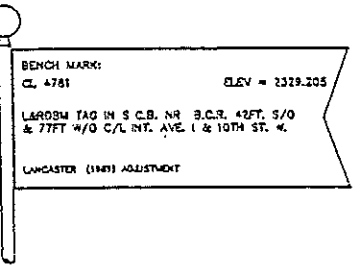
TYPICAL SECTIONS



ENGINEER'S NOTICE TO CONTRACTORS

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITY PIPES OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF THE AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE THERE ARE NO EXISTING UTILITIES EXCEPT THOSE SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN AND ANY OTHER LINES NOT OF RECORD OR NOT SHOWN ON THIS DRAWING. PRIOR TO EXCAVATION THE CONTRACTOR SHALL CALL UNDERGROUND SERVICE ALERT, TOLL FREE, AT 1-800-422-4133 TO VERIFY THE LOCATION OF UNDERGROUND UTILITIES.

JOHN DOE R.C.E. NO. 00000



LEGEND

- WATER MAIN
- TRAFFIC SIGNAL INTERCONNECT
- G.T.C.
- GAS MAIN
- SEWER MAIN
- EDGE OF EXISTING PAVEMENT
- EXISTING CHAIN LINK FENCE
- EXISTING CURB FACE
- SAWHOLE LINE
- COLD PLANE TO A DEPTH OF 6" MIN.
- SUM = SANITARY SEWER MANHOLE
- P.P. = POWER POLE
- C.L. = CENTER LINE
- T.C. = TOP OF CURB
- F.S. = FINISH SURFACE
- F.L. = FLOW LINE
- () = EXISTING ELEVATION
- DE = EXISTING
- CON = CONSTRUCTION
- P.C.C. = PORTLAND CEMENT CONCRETE
- CF = CURB FACE
- PT. = POINT
- STL = STATION
- CONFT. = CONSTRUCTION
- SOIL = SOIL
- D.R. = DEEMED RAMP
- S.C.R. = SLOPE CLOSE RETURN
- E.C.C. = 240 CURB RETURN
- A.P.W.A. = AMERICAN PUBLIC WORKS ASSOCIATION
- L.A.C.D.P.W. = LOS ANGELES COUNTY ROAD DEPARTMENT STANDARD PLANS
- SHT. = SHEET
- N.O. = NUMBER
- G.E. = GRADE BREAK
- AP.W.A. = AMERICAN PUBLIC WORKS ASSOCIATION
- L.A.C.D.P.W. = LOS ANGELES COUNTY ROAD DEPARTMENT STANDARD PLANS
- FIRE HYDRANT
- STREET SIGN
- WATER VALVE
- GAS VALVE
- POWER POLE

INDEX

- TITLE SHEET FOR STREET IMPROVEMENT PLANS
- 10TH STREET WEST STREET IMPROVEMENTS - FROM STA 28+00.00 TO STA 38+00.00
- 10TH STREET WEST STREET IMPROVEMENTS - FROM STA 38+00.00 TO STA 49+00.00
- 10TH STREET WEST STREET IMPROVEMENTS - FROM STA 49+00.00 TO STA 55+11.97
- MISCELLANEOUS PROPERTY IMPROVEMENT PLAN - FROM STA 28+00.00 TO STA 38+00.00
- MISCELLANEOUS PROPERTY IMPROVEMENT PLAN - FROM STA 49+00.00 TO STA 55+00.00
- PARKING LOT IMPROVEMENT PLAN FOR THE BEAUTY SCHOOL AND MR. PENN'S PROPERTY
- TITLE SHEET FOR STORM DRAIN PLANS
- 10TH STREET WEST STORM DRAIN PLAN - FROM STA 28+00.00 TO STA 38+00.00
- 10TH STREET WEST STORM DRAIN PLAN - FROM STA 38+00.00 TO STA 49+00.00
- 10TH STREET WEST STORM DRAIN PLAN - FROM STA 49+00.00 TO STA 55+11.97
- 10TH STREET WEST STORM DRAIN PLAN - NEWGROVE ST., NORBERRY ST., AND OLDFIELD ST.
- 10TH STREET WEST STORM DRAIN PLAN - LATERAL LINES
- 10TH STREET WEST STRIPING PLAN - FROM STA 30+00.00 TO STA 38+00.00
- 10TH STREET WEST STRIPING PLAN - FROM STA 38+00.00 TO STA 38+00.00

RECORD DRAWING

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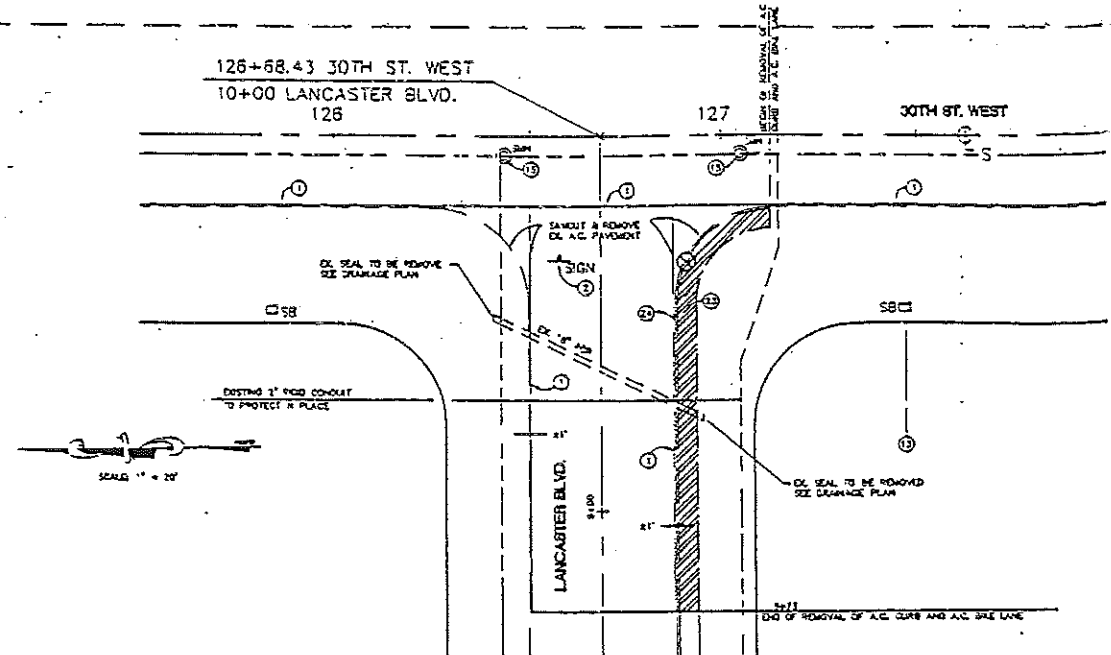
ALTERNATE LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYP. EACH SHEET)

PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. DOE FOR (CLIENT NAME AND COMPANY) COMPANY LOGO	RECORD NO.	REVISION BLOCK	PLANNING CITY/OUR ENGINEER	CITY OF LANCASTER	RECORD ENGINEER	DATE	CITY OF LANCASTER STREET IMPROVEMENTS PROJECT TITLE VICINITY MAP LEGEND GENERAL NOTES SHEET INDEX BENCH MARK CONST. NOTES
	REVISION APPROVAL DATE	REVISION DESCRIPTION	APPROVAL DATE	CHECKED BY: PLAN CHECK ENGINEER	CITY STAFF	DATE	
				REVIEWED: CITY ENGINEER	DIRECTOR OF PUBLIC WORKS	DATE	

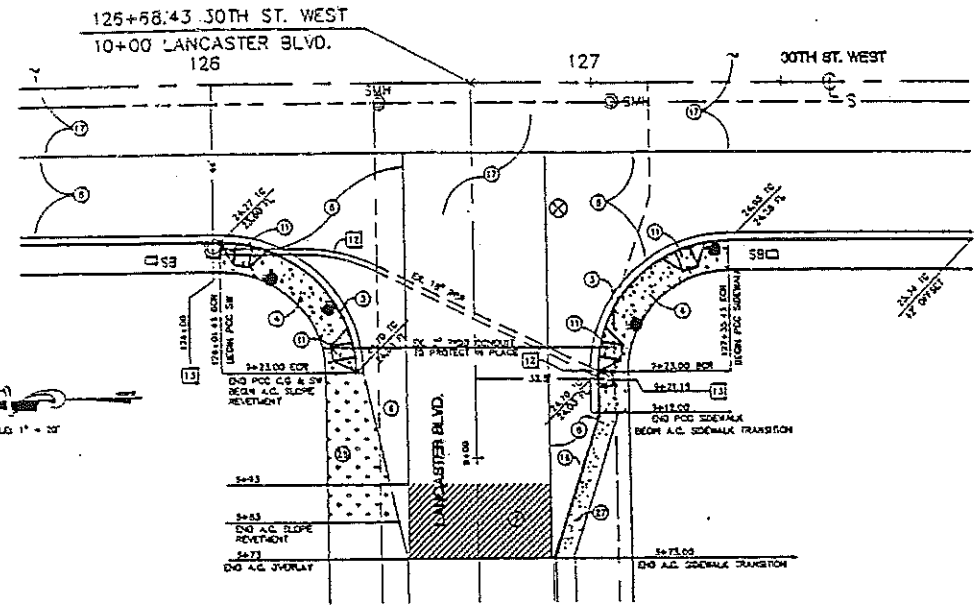
(24" MAX.)

PMT. 00-0000

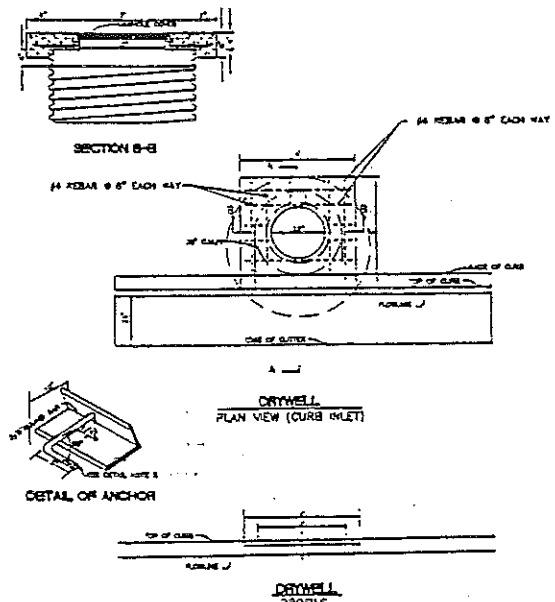
(36" MAX.)



TRANSITION AT LANCASTER BLVD
REMOVAL

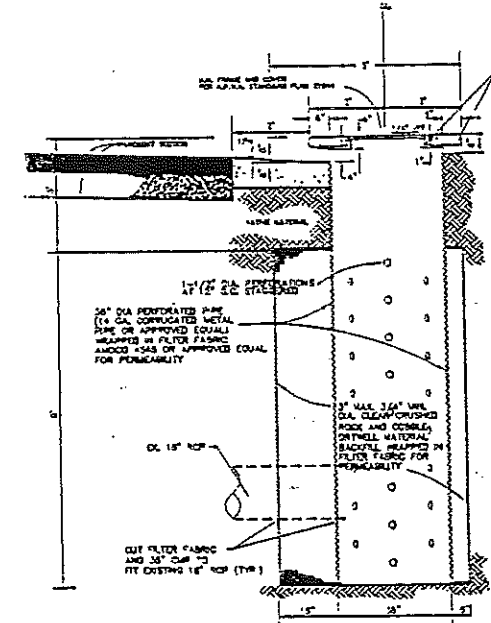


TRANSITION AT LANCASTER BLVD
CONSTRUCTION



NOTES:
1. FLOOR OF BOX TO BE TROWEL SMOOTH.
2. A HEADED STEEL STUD 3/8" x 3/8" WITH HEAD DOWN ATTACHED BY A FULL PENETRATION WELD MAY BE USED AS AN ALTERNATE ANCHOR.
3. THE 1" LEG OF THE INTERIOR ANCHORS SHALL BE PARALLEL TO THE TOP OF THE SIDEWALK.
4. 3 ANCHORS REQUIRED.

STANDARD DRYWELL



STANDARD DRYWELL (CURB INLET)
SECTION A - A

ALTERNATE LOCATION &
FORMAT OF CITY
SIGNATURE BLOCKS
(TYP. EACH SHEET)

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AS TO COMPLETENESS OR
ACCURACY OF DATA
INCORPORATED INTO THESE
DRAWINGS AS A RESULT OF
WORK BY OTHERS.

PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. JOE FOR
(CLIENT NAME AND COMPANY)
COMPANY LOGO
SIGNATURE: _____ DATE: 20-00-00
REV: _____ EXP DATE: _____



RECORD NO.	REVISION DESCRIPTION	PLANNING CITY ENGINEER	APPROPRIATE DATE

CITY OF LANCASTER		RECORD ENGINEER	
CHECKED BY: PLAN CHECK ENGINEER	DATE	RECORD ENGINEER	DATE
REVIEWED: CITY ENGINEER	DATE	CITY STAFF	DATE
APPROVED: DIRECTOR OF PUBLIC WORKS	DATE	RECORD DRAWING	DATE

CITY OF LANCASTER
STREET IMPROVEMENT PLAN
PROJECT TITLE
MISCELLANEOUS DETAILS
SCALE: NONE 1 CAD DWG FILE NO.

SHEET
2
X
OF
215

PMT. 00-0000

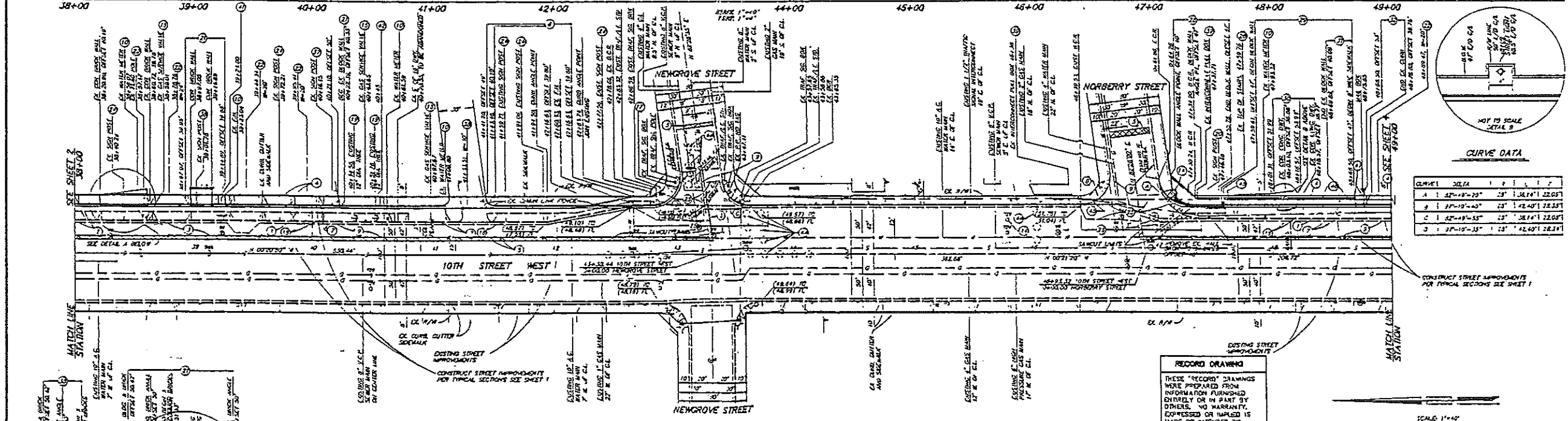
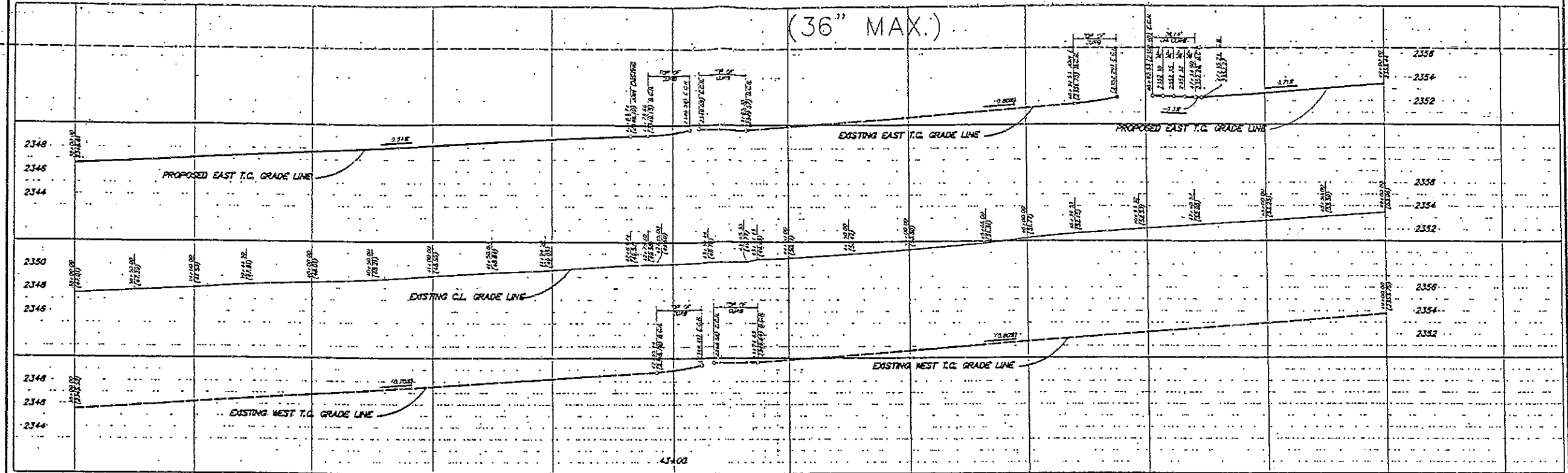
(24" MAX.)

(24" MAX.)

(36" MAX.)

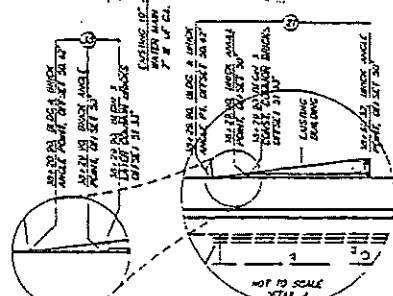
CREATED BY PLAN CHECK ENGINEER	DATE	REVIEWED BY CITY ENGINEER	DATE	APPROVED BY DIRECTOR OF PUBLIC WORKS	DATE
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PREFERRED LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYPICAL EVERY SHEET)



CURVE DATA

CURVE #	BEARING	ANGLE	PI	PC	PT	PT	PI
A	27°-19'-25"	25°	1+24.11'	22.65'			
B	27°-19'-25"	25°	1+24.07'	22.57'			
C	27°-19'-25"	25°	2+21.14'	22.65'			
D	27°-19'-25"	25°	2+21.14'	22.57'			



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ALTERNATE LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYP. EACH SHEET)

PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. DOE FOR
(CLIENT NAME AND COMPANY)
COMPANY LOGO
SIGNATURE: _____ DATE: _____



REVISION	REVISION DESCRIPTION	PLANNED BY	CITY ENGINEER	APPROVED BY	DATE

RECORD ENGINEER	DATE	CITY STAFF	DATE	RECORD DRAWING	DATE

CITY OF LANCASTER
STREET IMPROVEMENTS
10TH STREET WEST
FROM STA. 38+00.00 TO STA. 49+00.00
SHEET 3 OF X
SCALE: NONE | CAD DWG FILE NO.

(36" MAX.)

GENERAL NOTES FOR STREET LIGHTING LAYOUT

- STREET LIGHT LOCATIONS MAY BE ADJUSTED IN THE FIELD A MAXIMUM OF 10' TO AVOID EXISTING OBSTRUCTIONS SUCH AS DRIVEWAYS, CATCH BASINS, FIRE HYDRANTS, ETC. ANY DEVIATION EXCEEDING 10' SHALL BE APPROVED BY THE CITY OF LANCASTER'S DEPARTMENT OF PUBLIC WORKS SUBDIVISION SECTION.
- A REVISED STREET LIGHTING PLAN SHALL BE REQUIRED FOR ANY CHANGES MADE TO THE SUBDIVISION MAPS OR DESIGN PLANS WHICH AFFECT STREET ALIGNMENTS, LOT SIZES, PARCEL SIZES, BOUNDARIES, ETC.
- IN ORDER FOR THE CITY OF LANCASTER'S LIGHTING DISTRICT TO ASSUME THE OPERATIONS AND MAINTENANCE OF A LIGHTING SYSTEM ON ANY PUBLIC OR PRIVATE STREET, THE STREET SHALL BE OPEN TO THE GENERAL PUBLIC.
- ON STREETS WHERE THE SIDEWALKS ARE FIVE FEET OR LESS IN WIDTH, EXCLUDING THE TOP OF CURB, AND ARE ADJACENT TO THE CURB, THE STREET LIGHTING ELECTRODER STANDARDS AND THE LAMPS SHALL BE PLACED OUTSIDE THE SIDEWALK AREA UNLESS OTHERWISE SPECIFIED ON THE CITY OF LANCASTER APPROVED PLAN.
- ALL MAST ARMS AND BRACKETS SHALL BE PERPENDICULAR TO THE CURB FACE AND 6' LONG UNLESS OTHERWISE SPECIFIED ON THE CITY OF LANCASTER APPROVED PLAN.
- THIS STREET LIGHT LAYOUT EXPIRES IN TWO YEARS FOR ALL SUBDIVISIONS (ONE YEAR FOR NON-SUBDIVISIONS) AFTER THE LATEST APPROVAL DATE.
- ALL LIGHTS SHOWN ON THIS PLAN SHALL BE INSTALLED AND OPERATIONAL PRIOR TO THE ACCEPTANCE OF THE SYSTEM BY THE CITY OF LANCASTER'S LIGHTING DISTRICT.
- ALL STREET WIRING AND APPURTENANT APPARATUS SHALL BE UNDERGROUND.
- STREET LIGHTS SHALL BE CONSTRUCTED PER THE CITY OF LANCASTER APPROVED PLAN. LIGHTS NOT CONSTRUCTED ACCORDING TO THE APPROVED PLAN SHALL BE REMOVED AND RELOCATED AT NO EXPENSE TO THE CITY OF LANCASTER.
- WHEN IMPROVEMENT PLANS HAVE BEEN SUBMITTED TO THE CITY FOR CHECKING AND THE CHECKING PROCESS HAS BEEN INTERRUPTED FOR A PERIOD OF ONE YEAR OR MORE, THE PLANS SHALL BE DEEMED ABANDONED. APPROVED IMPROVEMENT PLANS SHALL BE DEEMED ABANDONED IF CONSTRUCTION HAS NOT COMMENCED WITHIN TWO YEARS OF THE LATEST APPROVAL DATE. IF CONSTRUCTION IS INTERRUPTED FOR A PERIOD OF ONE YEAR OR MORE, THE PLANS SHALL BE DEEMED ABANDONED. ABANDONED PLANS SHALL BE RESUBMITTED FOR REVIEW AND ALL FEES SHALL BE PAID IN ACCORDANCE WITH THE CITY'S ABANDONED PLANS POLICY PRIOR TO ANY PERMITS BEING ISSUED.

THE FOLLOWING ARE BASED ON I.E.S. DESIGN CRITERIA:

THE 9,500 LUMEN POLE SPACING IS BASED ON 0.4 FOOTCANDLE FOR LOCAL RESIDENTIAL STREETS. THE LUMINAIRE SHALL HAVE A 30 FOOT MOUNTING HEIGHT, SHALL BE (A.N.S.I.-I.E.S.) FULL - CUTOFF TYPE III AND EQUIPPED WITH A 100 WATT, HIGH PRESSURE SODIUM VAPOR LAMP.

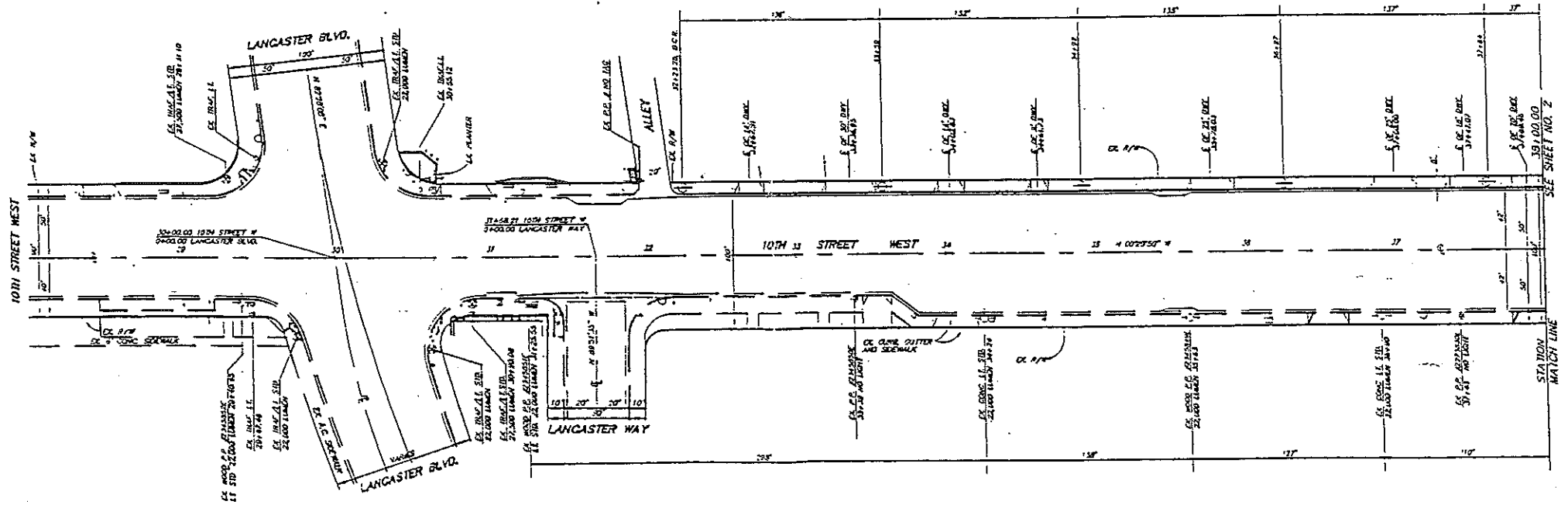
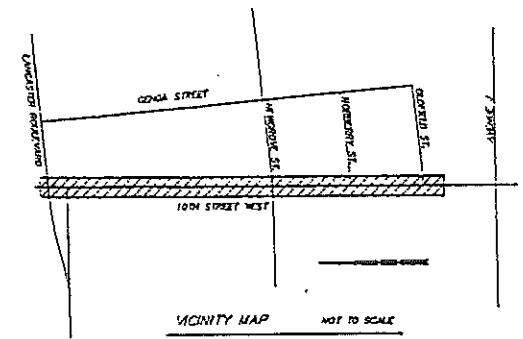
THE 22,000 LUMEN POLE SPACING IS BASED ON 1.4 FOOTCANDLE FOR MAJOR INTERMEDIATE STREETS. THE LUMINAIRE SHALL HAVE A 30 FOOT MOUNTING HEIGHT SHALL BE (A.N.S.I.-I.E.S.) DROPPED GLASS TYPE III AND EQUIPPED WITH A 200 WATT, HIGH PRESSURE SODIUM VAPOR LAMP.

THE 27,500 LUMEN POLE SPACING IS BASED ON 1.8 FOOTCANDLE FOR MAJOR INTERMEDIATE STREETS. THE LUMINAIRE SHALL HAVE A 30 FOOT MOUNTING HEIGHT SHALL BE (A.N.S.I.-I.E.S.) DROPPED GLASS TYPE III AND EQUIPPED WITH A 250 WATT, HIGH PRESSURE SODIUM VAPOR LAMP.

CITY OF LANCASTER
10TH STREET WEST STREET LIGHTING PLAN
FROM LANCASTER BOULEVARD TO OLDFIELD STREET

LEGEND

- 1 EACH ● PROPOSED 9,500 LUMEN, 100 WATT H.P.S. LAMP ON CONCRETE POLE.
- 17 EACH ○ PROPOSED 22,000 LUMEN, 200 WATT H.P.S. LAMPS ON CONCRETE POLE.
- EXISTING HIGHWAY SAFETY LIGHT (H.S.L.), TO REMAIN.
- EXISTING STREET LIGHT - TYPE OF POLE, SIZE OF LAMPS, AND POLE NUMBER AS INDICATED.
- 5 EACH ○ EXISTING STREET LIGHT - TYPE OF POLE, SIZE OF LAMPS, AND POLE NUMBER AS INDICATED.
- EXISTING UTILITY POLE - TYPE OF POLE AND POLE NUMBER AS INDICATED.



(24" MAX.)

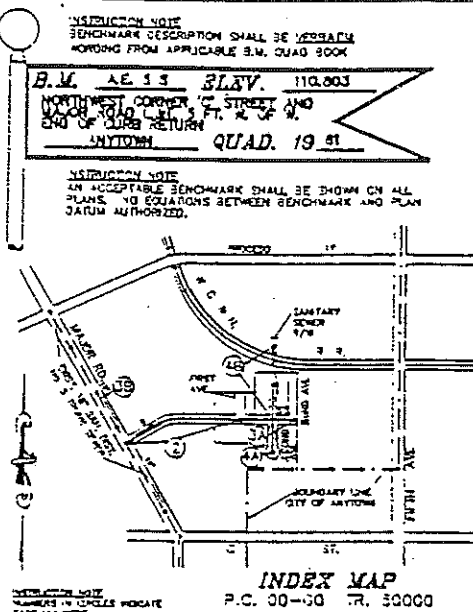
SEE CURRENT CITY ENGINEERING DEPARTMENT SUBMITTAL REQUIREMENTS PACKET IN THE CITY ENGINEERING DEPARTMENT

ALTERNATE LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYP. EACH SHEET)

PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. JOE FOR (CLIENT NAME AND COMPANY) COMPANY LOGO (SIGNATURE)		RECORD RCE	REVISION BLOCK REVISION DESCRIPTION	PLAN CITY OR ENGINEER	CITY OF LANCASTER		
		REV#1 APPR DATE	APPR APPR DATE	CHECKED BY: PLAN CHECK ENGINEER	DATE	STREET IMPROVEMENTS PROJECT TITLE STREET LIGHTING PLAN	SHEET 1 X OF 1113
		APPROVED: DIRECTOR OF PUBLIC WORKS	DATE	REVIEWED: CITY ENGINEER	DATE	SCALE: NONE * CAD FILE #10	LGT - 000 PMT. 00-0000

DATE CITY ENGINEER R.C.E. NO. DATE CITY ENGINEER R.C.E. NO. DATE CITY ENGINEER R.C.E. NO.

"AS-BUILT" BASED UPON INFORMATION SUPPLIED BY THE CITY INSPECTOR AND THE SEWER CONTRACTOR (AND) FIELD REPORT



GENERAL NOTES: SEE CURRENT GENERAL NOTES IN THE CITY SEWER IMPROVEMENT PLAN SUBMITTAL REQUIREMENTS PACKET.

CONSTRUCTION NOTES: SEE CURRENT CONSTRUCTION NOTES IN THE CITY SEWER IMPROVEMENT PLAN SUBMITTAL REQUIREMENTS PACKET.

MISCELLANEOUS NOTES: PROFILE ALIGNMENT AND GRADE OF SANITARY SEWERS TO BE CONSTRUCTED IN THE CITY OF LANCASTER, CA TRACT NO. 50000 PRIVATE CONTRACT NO. 00-00 2 SHEETS; 4 PAGES

ENGINEER'S NOTICE TO CONTRACTORS THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITY PIPES OR STRUCTURES SHOWN ON THESE PLANS ARE OBTAINED BY A SEARCH OF AVAILABLE RECORDS...

Table with columns: PIPE SIZE, MINIMUM, SUB-MINIMUM. Rows include pipe sizes from 12" to 24" and their corresponding minimum and sub-minimum values.

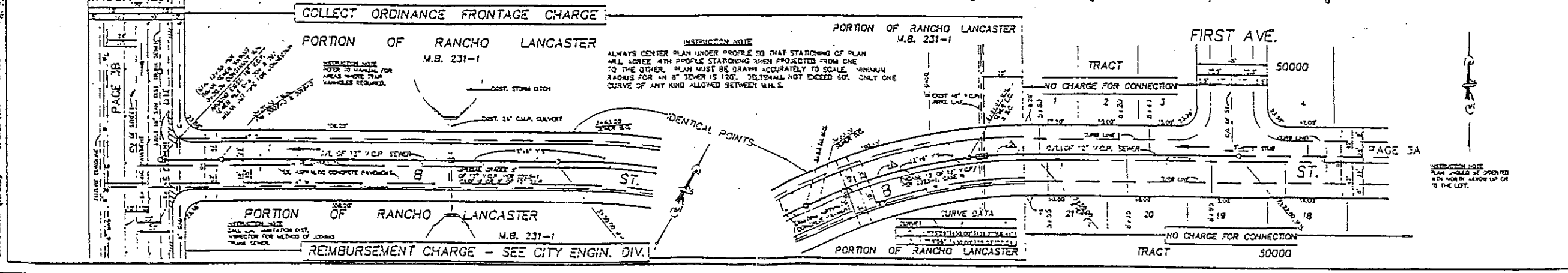
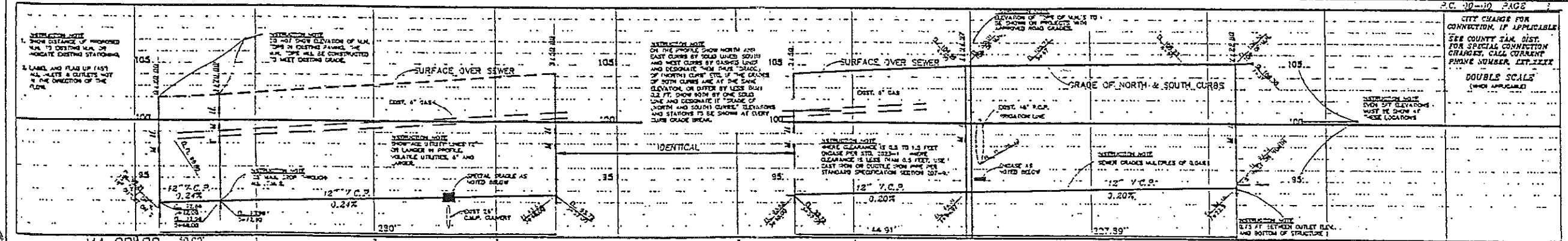
ENGINEER'S STATEMENT OF RESPONSIBILITY

THE ENGINEER SIGNING THESE PLANS IS RESPONSIBLE FOR THE ACCURACY AND ACCEPTABILITY OF THE WORK HEREON. IN THE EVENT OF DISCREPANCIES ARISING DURING CONSTRUCTION...

REVISION NO. DATE (DESCRIPTION OF REVISION IN FULL INCLUDING PAGE NUMBER(S) AND STATION(S) INVOLVED) APPROVED: DATE

DEPARTMENT OF PUBLIC WORKS CONSOLIDATED SEWER MAINTENANCE DISTRICT REVIEWED FOR MAINTENANCE REVIEWED BY DATE

CITY OF LANCASTER, CALIFORNIA JEFF L. LONG, DIRECTOR OF PUBLIC WORKS APPROVED BY COUNTY SANITATION DISTRICT NO. 14 OF LOS ANGELES COUNTY, CALIFORNIA



16 2000 B.C. 00-00

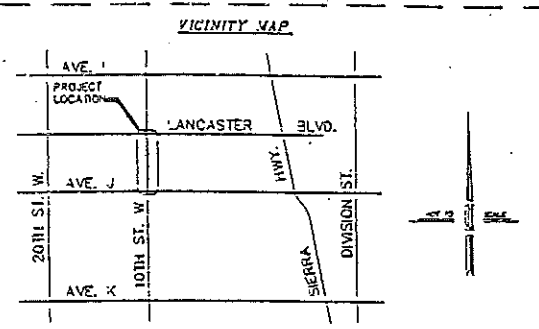
(24" MAX.)

(36" MAX.)

HYDRAULIC ELEMENT TABLE

MAIN	STATION TO STATION	PIPE SIZE/STRUCTURE	Q (cfs)
	XX+XX.XX	XX+XX.XX XX" RCP	XXX.X
	XX+XX.XX	XX+XX.XX XX" RCP	XXX.X
	XX+XX.XX	XX+XX.XX XX" RCP	XXX.X
	XX+XX.XX	XX+XX.XX XX" RCP	XXX.X
	XX+XX.XX	XX+XX.XX TRANSITION STRUCTURE	XXX.X
	XX+XX.XX	XX+XX.XX XX" RCP	XXX.X
	XX+XX.XX	XX+XX.XX XX" RCP	XXX.X
	XX+XX.XX	XX+XX.XX XX" RCP	XXX.X
LATERAL	A	XX+XX.XX XX" RCP	XX.X
	A	XX+XX.XX XX" RCP	XX.X
	A	XX+XX.XX XX" RCP	XX.X
	A	XX+XX.XX XX" RCP	XX.X
	A	XX+XX.XX XX" RCP	XX.X
	A	XX+XX.XX XX" RCP	XX.X

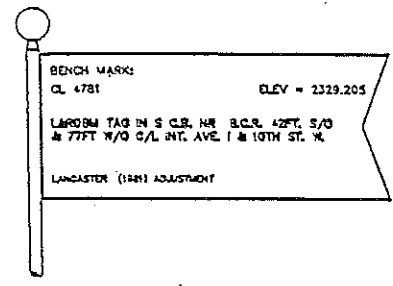
CITY OF LANCASTER
10TH STREET WEST STORM DRAIN PLAN
 BETWEEN AVENUE J AND LANCASTER BOULEVARD



CONSTRUCTION NOTES:

- CONSTRUCT REINFORCED CONCRETE COLLAR FOR L.A.C.F.D. (TYPICAL) DRAWING NO. 2-0183
- CONSTRUCT CATCH BASIN NO. 2 FOR L.A.C.F.D. DWS. NO. 2-0182 AND NO. 2-0172
- CONSTRUCT LOCAL DEPRESSION FOR A.R.E.A. STANDARD PLAN 313-0 CASE "A" (EACH 5' 0" AND 7' 0" = 5' 7" + 4").
- INSTALL 48" DIAMETER C.L.P.C.P.
- CONSTRUCT MANHOLE NO. 4 FOR L.A.C.F.D. DWS. NO. 2-0113 (01-NORTH, 02-SOUTH)
- CONSTRUCT JUNCTION STRUCTURE NO. 4 FOR L.A.C.F.D. DWS. NO. 2-0185
- INSTALL 36" DIAMETER C.L.P.C.P.
- REMOVE EXISTING CONCRETE SEAL AND JOIN EXISTING STORM DRAIN.
- INSTALL 24" DIAMETER R.C.P.
- INSTALL 18" DIAMETER R.C.P.
- CONSTRUCT (MODIFIED) CATCH BASIN NO. 7 (WITH 2 GRATES) FOR L.A.C.F.D. DWS. NO. 2-0170 AND NO. 2-0172 AND REMOVABLE PROTECTION BAR FOR DWS. NO. 2-0173 ALONG WITH WELDING A 3/4" OR 1" DIAMETER BAR BETWEEN CENTER SUPPORT ASSEMBLY AND FACE PLATE. INSTALL 2 BICYCLE PROOF GRATES FOR CALTRANS DETAIL 0170, TYPE 10-03 AND 2-1003 GRATE.
- CONSTRUCT CATCH BASIN NO. 5 FOR L.A.C.F.D. DWS. NO. 2-0184 AND NO. 2-0172
- CONSTRUCT MANHOLE NO. 4 FOR L.A.C.F.D. DWS. NO. 2-0113 (01-NORTH, 02-SOUTH)
- CONSTRUCT MANHOLE NO. 1 FOR L.A.C.F.D. DWS. NO. 2-0102
- CONSTRUCT MANHOLE NO. 4 FOR L.A.C.F.D. DWS. NO. 2-0113 (01-NORTH, 02-SOUTH)

SHOW TO SCALE SCHEMATIC MAP OF EXISTING AND PROPOSED STORM DRAIN FACILITIES WITH SIZE OF LINE LABELING, DIRECTION OF FLOWS, STREET LAYOUTS, AND PAGE NUMBERING



LEGEND

— — — — —	WATER MAIN	CL = CENTER LINE
— — — — —	TV CABLE	TO = TOP OF CURB
— — — — —	G.T.E.	F.S. = FINISH SURFACE
— — — — —	S.C.E.	FL = FLOW LINE
— — — — —	GAS MAIN	(E) = EXISTING ELEVATION
— — — — —	SEWER MAIN	EX = EXISTING
— — — — —	EDGE OF EXISTING PAVEMENT	PT = POINT
— — — — —	EXISTING CHAIN LINK FENCE	STA = STATION
— — — — —	EXISTING CURB FACE	N = NORTH
— — — — —	SANITARY LINE	CONST. = CONSTRUCTION
		EXST. = EXISTING
		S.E.C.P. = SEWER CURB RETURN
		INV. = INVERT
		F.S. = FINISH SURFACE
		ED = EDGE OF GUTTER
		SHT. = SHEET
		MB = MANHOLE
		CB = CATCH BASIN
		VD = VIBRATOR DETECTOR
		FD = FIRE HYDRANT
		S = STREET SIGN
		GV = GAS VALVE
		WV = WATER VALVE
		P = POWER POLE

INDEX

- TITLE SHEET FOR STORM DRAIN PLANS
- MAIN LINE ALONG 10TH ST. W. - FROM STA 28+00.00 TO STA 35+00.00
- MAIN LINE ALONG 10TH ST. W. - FROM STA 38+00.00 TO STA 49+00.00
- MAIN LINE ALONG 10TH ST. W. - FROM STA 49+00.00 TO STA 58+13.97
- LATERAL LINES ALONG NEWGROVE ST., HORBERRY ST., AND OLDFIELD ST.
- LATERAL LINES: 11, 21, 31, 41, 51, 52, 61, 71, 72, 81, 91, AND 92

ENGINEER'S NOTICE TO CONTRACTORS

THE EXISTENCE AND LOCATION OF ANY UNDERGROUND UTILITY PIPES OR STRUCTURES SHOWN ON THESE PLANS WERE OBTAINED BY A SEARCH OF THE AVAILABLE RECORDS. TO THE BEST OF OUR KNOWLEDGE THERE ARE NO EXISTING UTILITIES EXCEPT THOSE SHOWN ON THESE PLANS. THE CONTRACTOR IS REQUIRED TO TAKE DUE PRECAUTIONARY MEASURES TO PROTECT THE UTILITY LINES SHOWN AND ANY OTHER LINES NOT OF RECORD OR NOT SHOWN ON THIS DRAWING. PRIOR TO EXCAVATION THE CONTRACTOR SHALL CALL UNDERGROUND SERVICE ALERT, TOLL FREE, AT 1-800-422-4133 TO VERIFY THE LOCATION OF UNDERGROUND UTILITIES.

JOHN Q. DOE R.C.E. NO. 000000

STORM DRAIN GENERAL NOTES

SEE CURRENT STORM DRAIN GENERAL NOTES IN THE CITY STORM DRAIN IMPROVEMENT PLAN SUBMITTAL PACKET

RECORD DRAWING
 THESE "RECORD" DRAWINGS WERE PREPARED FROM INFORMATION FURNISHED ENTIRELY OR IN PART BY OTHERS. NO WARRANTY, EXPRESSED OR IMPLIED IS MADE OR INTENDED BY THE ENGINEER OF RECORD AS TO COMPLETENESS OR ACCURACY OF DATA INCORPORATED INTO THESE DRAWINGS AS A RESULT OF WORK BY OTHERS.

PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. DOE FOR (CLIENT NAME AND COMPANY)
COMPANY LOGO
 (SIGNATURE) DATE: 20-00-00
 (SCALE) EXP. DATE



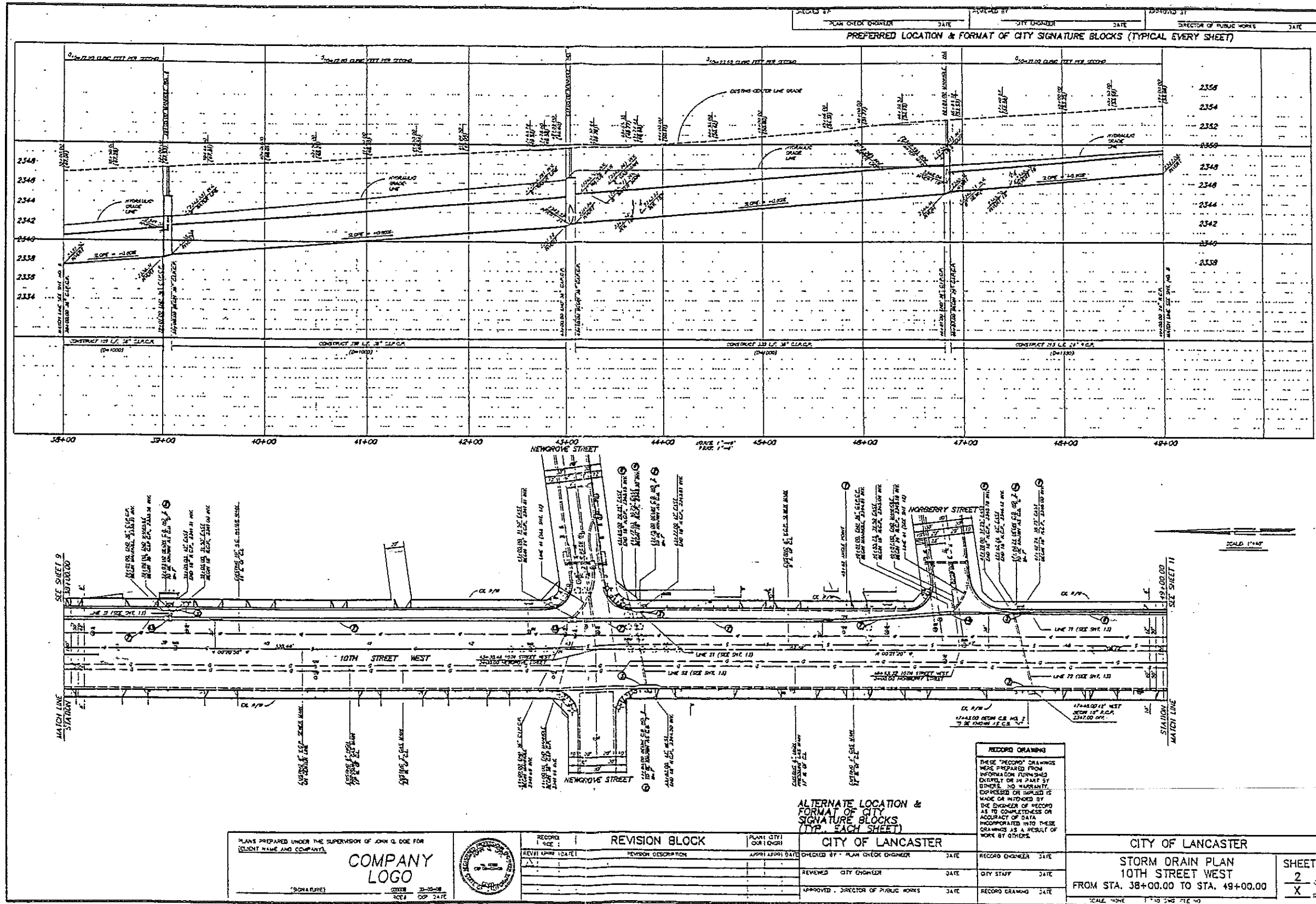
REVISION BLOCK	PLAN CITY OR ENGINEER	CITY OF LANCASTER
REVISION NO. DATE REVISION DESCRIPTION	APPROPRIATE DATE CHECKED BY PLAN CHECK ENGINEER DATE	RECORD ENGINEER DATE
	REVIEWED CITY ENGINEER DATE	CITY STAFF DATE
	APPROVED DIRECTOR OF PUBLIC WORKS DATE	RECORD DRAWING DATE

CITY OF LANCASTER		SHEET 1 OF X
STORM DRAIN PLAN PROJECT TITLE VICINITY MAP BENCH MARK GENERAL & INDEX MAP & SHEET INDEX ENG. NOTE CONST. NOTES		

PMT. 00-0000

(36" MAX.)

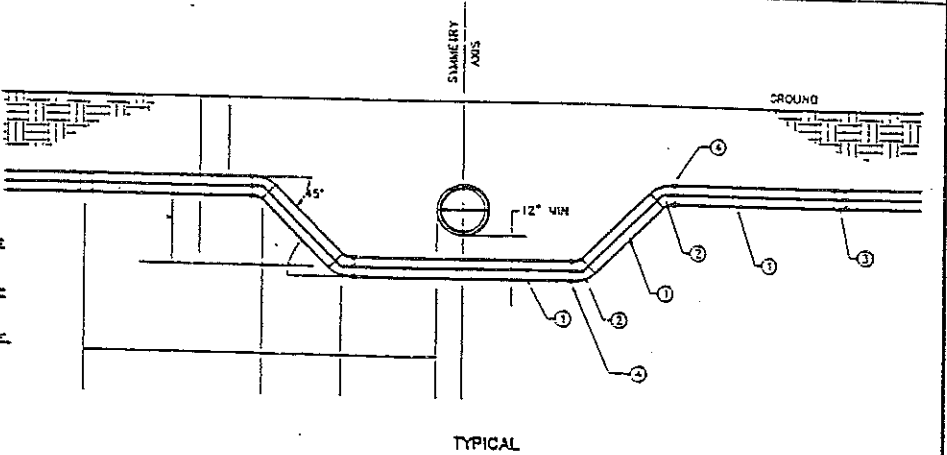
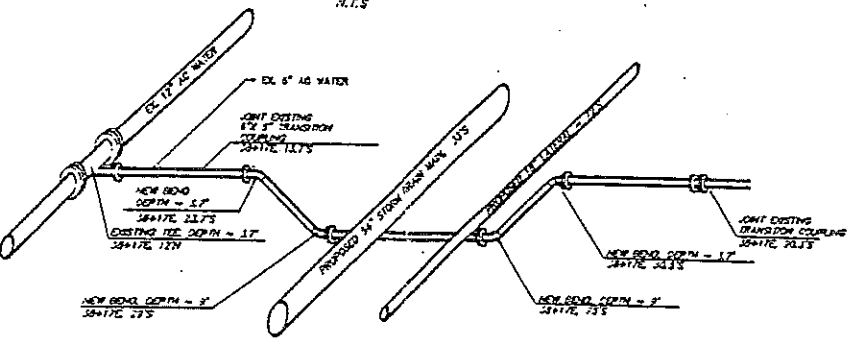
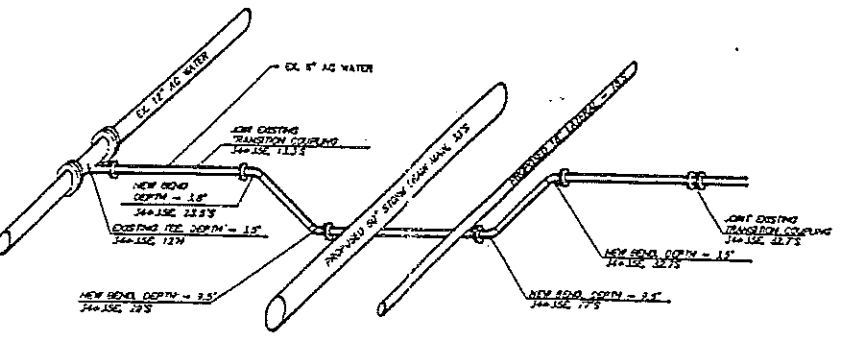
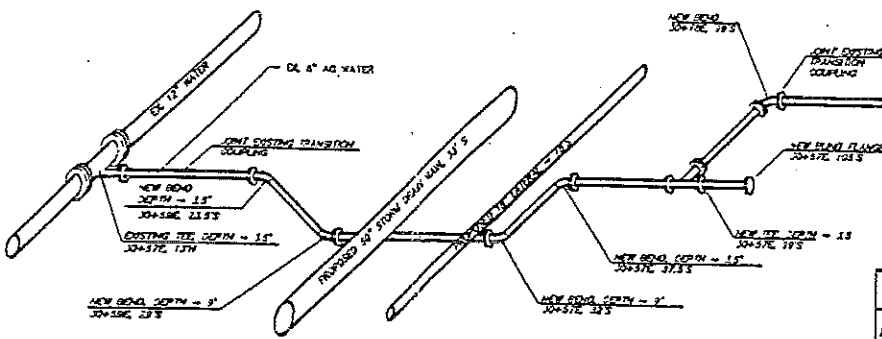
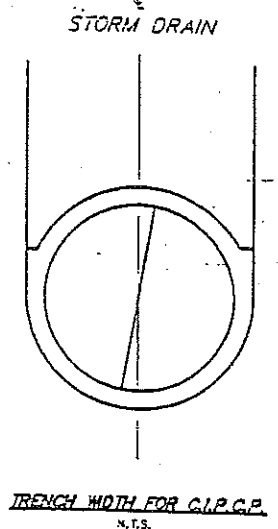
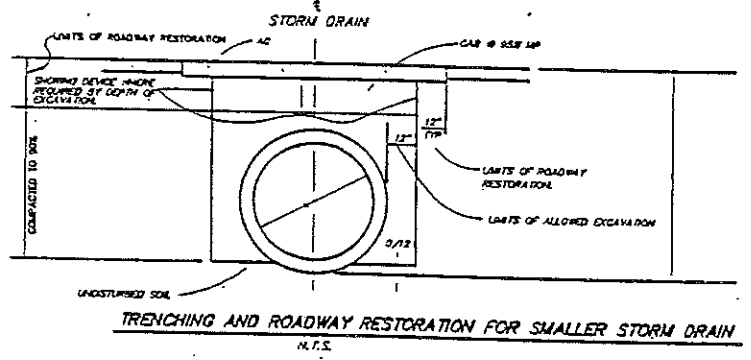
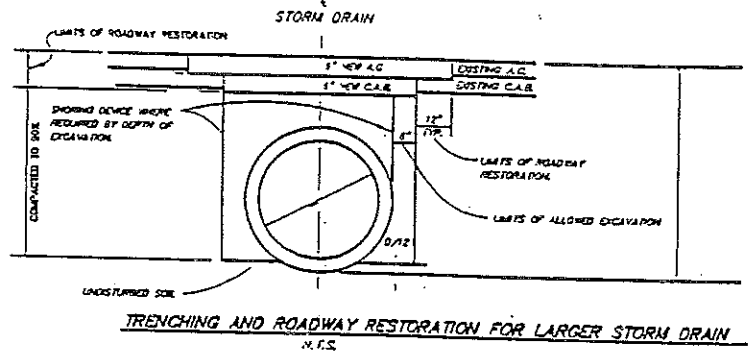
(24" MAX.)



STORM DRAIN PLAN/PROFILE SHEET (SAMPLE) - PLATE 10.1.12

PMT. 00-0000

(36" MAX.)



LOCATIONS OF RELOCATIONS									
ITEM	LOCATION	HS 1	SIZE	EX	NEW	SD	SD	ON	
		OW	TYPE		COV	NO	LINE		
1	20+100 1.5M	EW	12" O	TYPICAL	3.5	7.5	18	22	4
2	22+010 1.5M	EW	12" O	TYPICAL	3.5	7.0	18	23	2.5
3	22+250 1.5M	EW	12" O	TYPICAL	3.5	7.0	18	41	2.5
4	31+240 1.2M	EW	12" AC	TYPICAL	4.0	8.5	18	33	2.5
5	35+050 1.2M	EW	12" AC	TYPICAL	3.5	8.5	18	52	2
6	36+050 1.2M	EW	12" AC	TYPICAL	4.0	7.0	18	72	3
7	22+030 1.15	AS	6" AC	TYPICAL	3.5	10.0	60	MANH	0.5
8	30+570 1.15	AS	6" AC	SPECIAL	2.5	2.5	60M B	MANH/AT	6
9	34+130 1.15	AS	6" AC	SPECIAL	2.8	2.5	60M B	MANH/AT	5.7
10	38+170 1.15	AS	6" AC	SPECIAL	2.7	5.0	60M B	MANH/AT	5.3

BILL OF MATERIAL		
ITEM	DESCRIPTION	UNIT QTY
1	12" DIA STEEL PIPE, 1/4" THICK, CLASS CMC & CMC	LF 244
2	6" DIA STEEL PIPE, 1/4" THICK, CLASS CMC & CMC	LF 247
3	12"-45" STEEL ELBOW, 1/4" THICK, F BY M. CMC & CMC	EA 24
4	6"-45" STEEL ELBOW, 1/4" THICK, F BY M. CMC & CMC	EA 12
5	4"-45" STEEL ELBOW, 1/4" THICK, F BY M. CMC & CMC	EA 4
6	12" TRANSITION COUPLING, EPOXY LINED & COATED	EA 12
7	6" TRANSITION COUPLING, EPOXY LINED & COATED	EA 4
8	6" x 3" TRANSITION COUPLING, EPOXY LINED & COATED	EA 2
9	4" TRANSITION COUPLING, EPOXY LINED & COATED	EA 2
10	12" SLIP-ON STEEL WELD FLG. CL150	EA 14
11	6" SLIP-ON STEEL WELD FLG. CL150	EA 24
12	6" BLIND FLG. CL150	EA 1
13	6" x 8" x 6" STEEL TEE, 1/4" THICK, F BY M. CMC & CMC	EA 2
14	CONCRETE BLOCKING, AS NECESSARY	

NOTES:
 1. AT ALL POINTS OF INTERSECTION TO THE EXISTING WATER SYSTEM, THE CONTRACTOR MUST EXCAVATE FIRST TO DETERMINE ACTUAL DEPTH AND NEED FOR RELOCATION.
 2. THE CONTRACTOR SHALL VERIFY THE SIZE AND TYPE OF WATER PIPE AND FITTINGS ON WHICH OFFSET IS TO BE CONSTRUCTED BEFORE COMMENCING THE CONSTRUCTION.
 3. FOR 6" TO 12" AC OR STEEL TO 40 PPM TRANSITIONS, THE CONTRACTOR SHALL USE DRESSER STYLE 122, FLANGED ADAPTER, EPOXY LINED IN LCV OF 8 x 4 F ADAPTER.

PLANS PREPARED UNDER THE SUPERVISION OF JOHN Q. JOE FOR
 (CLIENT NAME AND COMPANY)
COMPANY LOGO
 (SIGNATURE) _____
 DESIGNED BY: _____
 CHECKED BY: _____
 DATE: _____



REVISION	NO.	DATE	DESCRIPTION
1	1		

ALTERNATE LOCATION & FORMAT OF CITY SIGNATURE BLOCKS (TYP. EACH SHEET)
CITY OF LANCASTER
 CHECKED BY: PLAN CHECK ENGINEER DATE: _____
 REVIEWED BY: CITY ENGINEER DATE: _____
 APPROVED BY: DIRECTOR OF PUBLIC WORKS DATE: _____

RECORD DRAWINGS
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CITY OF LANCASTER
 STORM DRAIN PLAN
 WATER LINE RELOCATIONS
 & MISCELLANEOUS DETAILS
 SHEET 3 OF X
 DATE: _____

(24" MAX.)