

CHAPTER 4 – DESIGN STANDARDS & GUIDELINES



4.0 Design Standards & Guidelines

To generate a vital, growing, sustainable economy, and support a health community, the Health District must become a compelling destination for patients, doctors, and other healthcare professionals, and all community members seeking healthy, active lifestyles. The combination of top-quality healthcare services, a central and easily accessible location and an attractive, engaging urban place has the potential to accomplish this goal, evolving the current under-developed and under-performing area into a place where people want to be; must as downtown Lancaster a decade ago made the rapid shift from deserted arterial thoroughfare to the BLVD District.

In competing with other healthcare enters and facilities, the Health District will present the first-time visitor with welcoming streetscapes, new state of the art facilities, unique public spaces and wellness services, convenient and attractive retail and restaurant amenities, and competitive market-rate in-town housing options. The design character and quality, and human scale and comfort, of this place must be high to help overcome its current image and reputational disadvantages. Phasing strategies that delivery early wins, unique facilities, and comfortable places will be important to shift perceptions and build momentum.

The standards and guidelines of this chapter work in tandem to help realize the vision in three distinct urban environments: the District Core, the District General, and the District Edge.

4.1 Purpose and Applicability

4.1.1 Purpose

This chapter regulates the development and uses of lots, buildings, and public space in the Health District in order to create a vibrant and human-scaled mixed-use district. This chapter seeks to generate the following outcomes:

- The development of appropriately-scaled buildings.
- New and retrofitted streets for vibrant social, medical, and commercial focal points.
- A diverse collection of high-quality housing choices.
- Enhanced neighborhood character.
- Walkable neighborhood patterns with a network of well-designed streets that are safe for pedestrians, bicyclists, and motorists.
- Well-managed shared parking that is treated as a public amenity.

4.1.2 Applicability

This chapter applies to all parcels, subdivisions, streets, civic spaces, buildings, and their uses identified in Figure 4.2: Regulating Plan. All new construction and redevelopment is required to comply with the standards of this section. For modifications to existing structures, see Table 4.1.2 and Lancaster Municipal Code (LMC) Section 17.xx.xxx.

- A. Regulating Plan. The Regulating Plan must be consulted in the preparation of subdivisions and the development of all sites. Required new streets and public open spaces are important components that are intended to generate a physical environment consistent with the vision described in Chapter 3.
 - a. Regulating Plan Maintenance. Immediately following any approvals that affect the Regulating Plan, the Regulating Plan shall be updated to reflect the changes.
- B. Zoning. The Lancaster Health District Master Plan replaces the existing zoning with a new zone (Mixed Use – Health District) and three sub-zones for all property and uses within the Lancaster Health District Master Plan boundaries. In the event of overlapping or conflicting requirements with this Code and other sections of the Lancaster Municipal Code, this Code shall prevail.
- C. Terms. Throughout this chapter, the following terms apply: “must” and “shall” denote a standard or requirement that has the force of zoning; “should” and “recommended” denote a guideline; “Planning Manager” means the Planning Manager or their designee; “Directors” means the Development Services Director and the Director of Parks and Recreation. For the purposes of this Code, “lot” and “Property line” refer to the area and lines to be used for site design, respectively. Any subdivision of property requires the approval of a tentative map.
- D. Site Plan Review. All development within the boundaries of this Plan area are subject to LMC Section 17.12.630.
- E. Development Intensity. Development is not limited by density or floor area because the standards of this chapter has been prepared specifically to implement the intended physical character described in the Vision Plan. The maximum development potential of this Plan is consistent with what is allowed by the General Plan.
- F. Physical Form and Character. The intended physical form and character of the Plan area is based on a variety of characteristics that vary in response to the needs of three distinct environments. Each of these environments has been translated into a district. Each district generates and supports the intended variety of physical characteristics and uses through specific standards and design guidelines.

4.1.3 Adjustments

In some instances, it may be practical to vary from the prescribed form in the zone. Adjustments shall run with the land and be binding on the property owners and its/their successor and assigns. Adjustments shall be processed in compliance with LMC Section 17.12.780 and subject to the following findings.

- A. There are special circumstances or exceptional physical characteristics applicable to the property including size, shape, topography, location or surroundings involved which are not generally applicable to other properties in the same vicinity with the same zoning.
- B. An adjustment (if authorized) will not constitute a grant of special privilege inconsistent with the limitations on other properties in the same vicinity with the same zoning.
- C. The strict application of the requirements sought to be reduced or increased would result in practical difficulties and unnecessary hardship which is inconsistent with the purpose of the requirements.
- D. Approval of the application will result in the need for less grading and disturbance of soils and natural vegetation.
- E. Approval of the application will not diminish the visual appearance of the property or neighborhood.

4.2 Regulating Plan & Zones

4.2.1 Regulating Plan

This section establishes the Regulating Plan for the Lancaster Health District. This Plan establishes the locations of the three new zoning districts, the character of each zoning district and the framework structure of the street and public space network. The precise location of the District Core zoning may shift within the Central District subregion to accommodate the precise location of the hospital. Figure 4.1 shows the approximate locations of the three zoning districts and is not meant to provide any other information. It does not convey the actual location of the roadway network, which can be found on Figure 3.2. The three zoning districts are:

- A. District Core (Pink): This district is centered on the new Antelope Valley Hospital. The hospital location may be finalized in a variety of locations, but as of 2019, it is most likely to be located near the westerly District boundary and several hundred feet south of Avenue J. In front of the hospital, is Wellness Square, surrounded by the new hospital, other medical facilities, support commercial uses, and shared parking facilities. 20 to 30 acres in size, this zone extends out from Wellness Square, encompassing a sizeable area for the new hospital, supporting medical office, and mixed-use general office or housing.
- B. District General (Blue): This district surrounds the District Core, providing opportunities for additional medical offices and laboratories, general office space, additional wellness-related and support commercial uses, hospitality facilities, and a range of housing options. This zone occupies the balance of the Plan Area within the large northeast block at the corner of 20th Street West and Avenue J, the area south of Avenue J-8, and the westerly portion of the area east of 15th Street West. Lots fronting Avenue J and 15th Street West are prioritized for medical offices and commercial uses.
- C. District Edge (Tan): This district provides a transition from the larger scale and more active environment of the District General to the existing single-family neighborhoods to the east of 15th Street West.
- D. New Street Connections and Trajectories. For sites encompassing two acres or more, applicants for any subdivision, new development, or redevelopment shall demonstrate compliance with the Large Site standards of Section 4.3 and consistency with the vision of Chapter 3. District transitions may only occur mid-block, behind lots.

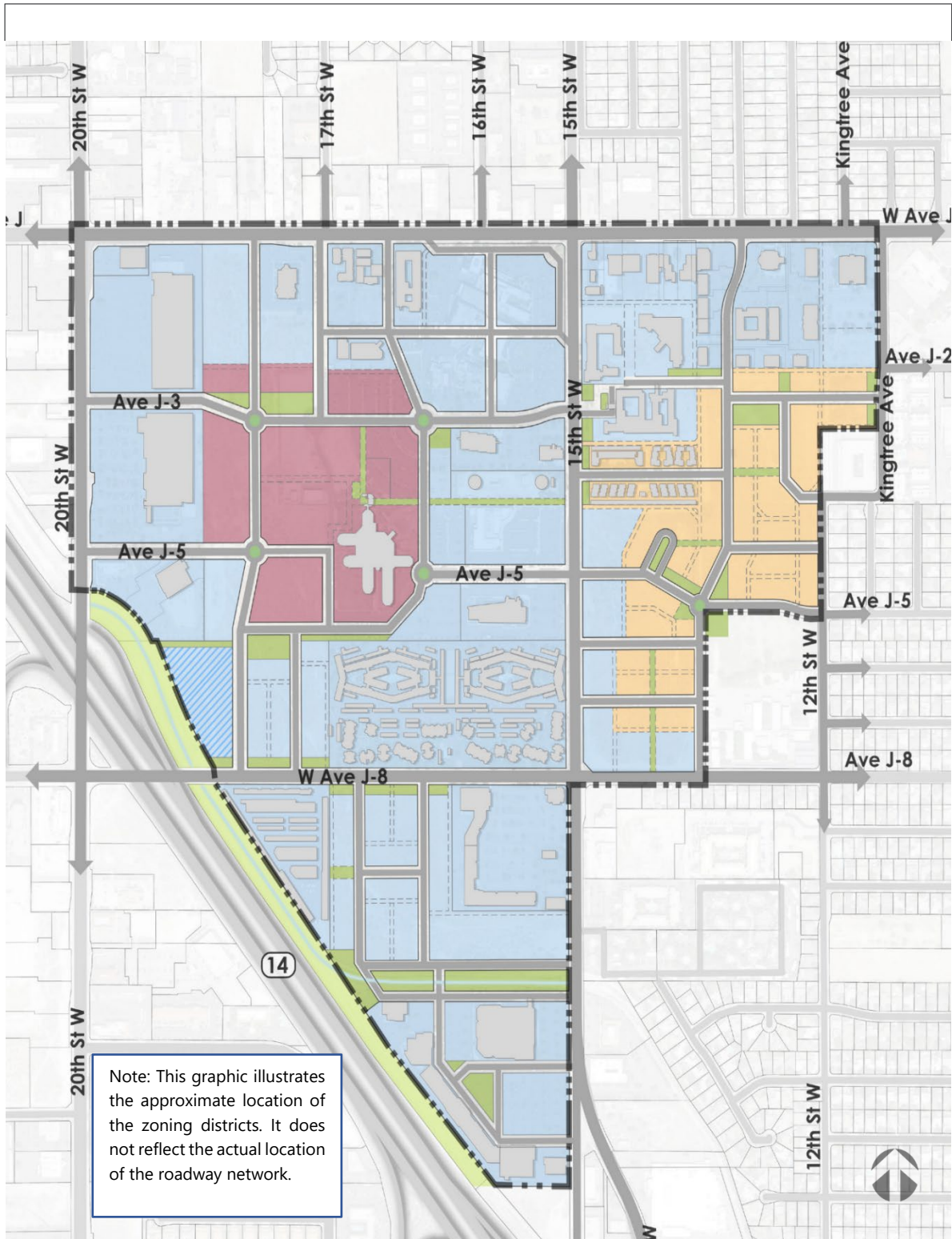


Figure 4.1, Regulating Plan

4.2.2 Districts

The Lancaster Health District Master Plan implements the community vision for the Plan Area by replacing the existing zoning with a new Mixed Use – Health District zone. Within the new zone are three new sub-districts/zones which are aimed at generating a particular physical environment, based on its location and role within the Lancaster Health District and in relation to the surrounding urban context.

District Core (DC). This district is characterized by active, pedestrian-oriented medical, retail, restaurant, art galleries, and other ground-floor uses set at or near the sidewalk, with market-rate housing and offices on the upper floors. Buildings have strong, welcoming ground-floor bases, upper floors with appropriately sun-shaded windows, and tops with distinctive profiles that integrate screening of rooftop equipment and/or provide roof terraces. Building heights typically range from one to six stories; hotels and hospitals do not have height limits.

District General (DG). This district is characterized by a flexible mixture of active residential and retail frontages, and both house-form and block-form buildings. Non-residential ground floor frontages are similar to those in the District Core area, while residential ground floors are raised and/or set back to provide residents with an appropriate amount of privacy, while giving them direct access to and views of the public realm. This enhances pedestrian comfort and safety within the public realm, which extends into semi-public open spaces in the form of shared courts and/or semi-private patios. Rooftop terraces are also encouraged to provide communal open spaces within these relatively densely-populated Lancaster Neighborhoods.

District Edge (DE). This is the least intense of the three new zoning districts. It abuts existing single-family neighborhoods, and is characterized primarily by single-family and house-form multi-family residential buildings. Live/work units and small neighborhood-serving commercial businesses are also allowed, but only to the extent that they are in scale and character with this primarily residential zone. Buildings are limited to two-story scale along street frontages, and are set back with landscaped front yards, porches, and stoops. Third floor spaces may be provided providing they are consistent with the predominantly two-story neighborhood scale.

DISTRICT CORE (DC)



Intent

To provide vibrant, walkable urban main street areas that provide locally- and regionally-serving medical, commercial, retail, entertainment, and civic uses.

Desired Form

- Building Size: block-form
- Buildings: attached; some detached
- Lot Widths: medium to large
- Building Footprints: medium to large
- Front/Side Setbacks: small to none
- Useable on-site open space: balconies, roof terraces
- Rear setbacks: Minimal
- Building height: up to 6 stories, excluding hospitals, hotels
- Ground floor: flush with sidewalk; may be elevated on side streets
- Parking Location: behind or under buildings

Streetscapes

Wide sidewalks for outdoor dining; trees in grates, sidewalks lined by tall shopfronts with entries facing the street, on-street parking.

General Use

Ground-floor active medical office, retail, and entertainment with housing and office on upper stories.

DISTRICT GENERAL (DG)



Intent

To provide a flexible, varied mix of commercial buildings and urban housing in medium- to large-footprint buildings with frontages that foster a walkable area and active community.

Desired Form

- Building Size: block-form with some house-form
- Buildings: attached and detached
- Lot Widths: medium to large
- Building Footprints: medium to large
- Front/Side Setbacks: small to medium
- Useable on-site open space: courts, balconies
- Rear setbacks: small
- Building height: up to 4 stories
- Ground floor: elevated except where ground floor non-residential, then flush with sidewalk
- Parking Location: behind or under buildings

Streetscapes

Ground floor entries face the street; trees in planters, wide sidewalks, on-street parking

General Use

Office and residential with some retail and service uses.

DISTRICT EDGE (DE)



Intent

To provide a variety of urban housing choices in small- to medium-footprint buildings that reinforce the walkable nature of the neighborhood and support public transportation and neighborhood-serving retail and service uses.

Desired Form

- Building Size: house-form
- Buildings: attached and detached
- Lot Widths: narrow to medium
- Building Footprints: narrow to medium
- Front/Side Setbacks: small to medium
- Useable on-site open space: courts, rear yards, balconies
- Rear setbacks: medium
- Building height: up to 3 stories
- Ground floor: elevated (porches, stoops, door yards)
- Parking Location: mid-lot and behind buildings

Streetscapes

Ground floor entries face the street; primarily residential ground floors; trees in planters, wide sidewalks, on-street parking.

General Use

Residential with live/work and home businesses.

4.3 Large Site Subdivisions

4.3.1 Overview

To effectively generate the physical form of the Health District as envisioned in Chapter 3, it is important that large sites be broken up into blocks that are appropriately sized for the intended environment. For the District to be urban and pedestrian-oriented, it is imperative that a high level of connectivity be created through the subdivision and site development process for large sites. This creates shorter walking distances for those on foot. See Block Standards below for block design strategies.

4.3.2 Applicability

The standards of this section apply to all parcels equal to or greater than two acres. Before the approval of any tentative map or the issuance of any building permit related to new or redevelopment¹ of the site, the applicant shall demonstrate compliance with the standards of this section. Applicants shall follow the procedures for a Site Plan Review as prescribed in Title 17 of the LMC. Required Findings:

- A. The site design is driven by the vision for the Health District contained in Chapter 3 of this Master Plan.
- B. Pedestrian comfort and convenience are prioritized in the laying out of new thoroughfares, lotting, and building placement.
- C. Proposed building footprints form generally continuous street walls, and are oriented towards the public realm.

4.3.3 Typical Block Standards

- A. What is considered a block? For the purposes of this Plan, “blocks” are considered groups of lots surrounded on all sides by pedestrian rights-of-way. Two of these rights-of-way must be thoroughfares identified in Section 4.10. The remaining two may be some form of Public Open Space (see Section 4.11). Block perimeters are measures from property lines and sides of Public Open Spaces. Alleys and Fire Lanes shall not count as block sides. See Table 4.10.

¹ For the purposes of this Code, “redevelopment” shall include any project that either demolishes 50 percent or more of the existing floor area of the primary building on site, or adds an amount of floor area that is equal to or more than 50 percent of the existing floor area of the primary building on site.

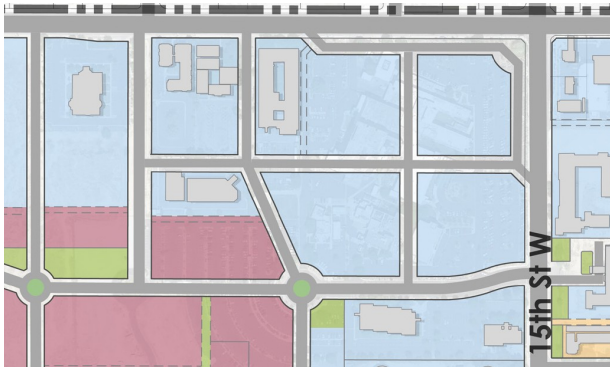
- B. Dead-End Streets. Dead-end streets (including cul-de-sacs) and dead-end alleys are not allowed, unless terminating at the edge of the project site or determined to be necessary by the City’s Traffic Engineer for operational purposes. In such cases a notation shall be included in the project regulating plan that all stubs shall connect with future thoroughfares on adjoining undeveloped property.
- C. New Lots. New blocks shall be organized into groups of lots dimensioned according to the intended development. These lot lines shall be recorded, and shall be used to demonstrate compliance with applicable standards (see Section 4.4).
- D. New lot shape and orientation. Through-lots and flag lots are strongly discouraged. Lots shall front onto an Active Street, Public Open Space (see Section 4.11) or allowed On-site Open Space (see Section 4.8). A rear lot line shall never abut a street.

Table 4.1
Block Sizes

Use	Depth	Perimeter
Commercial/Mixed Use	470	1,600
Residential	300	1,200

1

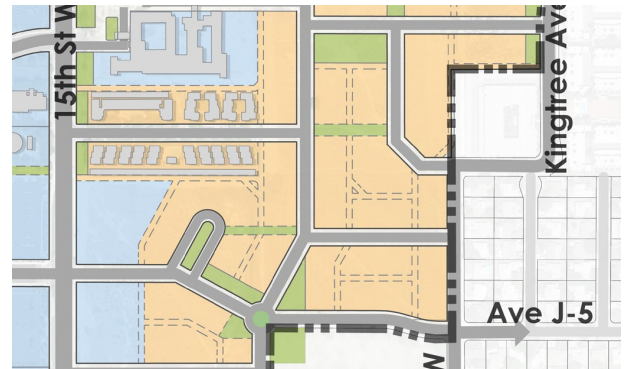
Create Urban-Scaled Blocks



Subdivide large sites into urban blocks, per the block dimension standards in Table 4.1 and the Lot Size Standards in Section 4.4. The arrangement of thoroughfares must align with those on adjacent sites, whether existing or proposed. Block size is measured from lot lines and the sides of Public Open Spaces. Only two sides per block may abut Public Open Spaces.

2

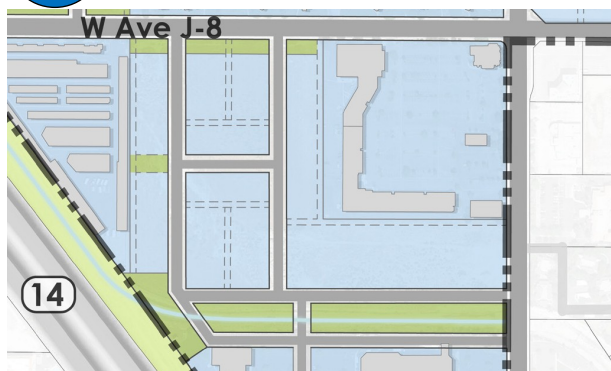
Introduce Streets & Alleys



Assign appropriate Thoroughfare Types selected from the allowed types in Section 4.10. Dead-end streets and cul-de-sacs are not generally allowed. Introduce alleys (privately maintained) to provide site access without the interruption of driveways along the streetscape.

3

Locate Public Open Spaces



Locate one or more areas that will work well for public open space. A minimum of ten percent of the net area (after all right-of-way space is subtracted) must be provided in the form of one or more of the allowed public open space types. This is in addition to Private Open Space requirements.

4

Draw Lots & Building Footprints



Draw lot lines. These shall be recorded and dimensioned according to the intended building type(s). Draw building footprints in compliance with all applicable standards.

4.4 Development Standards

4.4.1 General to All Zones

- A. Blocks. Before permits may be issued for the development or redevelopment² of any lot greater than two acres, the applicant shall demonstrate through the Site Plan Review process, consistency with the intent and standards of Section 4.3, Large Site Standards.
- B. Lot Sizes and Orientation. Any parcel that exceeds the maximum dimensions allowed within the zone shall, prior to new or redevelopment, re-lot the property in accordance with the standards of the applicable district. New lot lines shall be recorded, and must be used to demonstrate compliance with the standards of the applicable district and all other applicable standards. All newly drawn lots shall front an Active Street or Public Open Space. Newly drawn lots may not front onto access drive or alley.
- C. Building Orientation and Access. All street-adjacent buildings must orient toward, and take primary pedestrian access from the street. The ground-floor facades of these buildings shall parallel primary- and side-street lot lines and meet the setback lines for a minimum of specified percentage of the lot width and, in the case of corner lots, depth. Buildings and ground floor units (residential or commercial) that are not adjacent to a street must orient toward, and take primary pedestrian access from one of the following: court, paseo, green, side yard, or rear yard. The space onto which a building fronts must provide direct pedestrian access to the public right-of-way. Buildings and ground floor units (residential or commercial) may never orient toward, or take primary pedestrian access from a parking lot, access drive, fire lane, or alley.
- D. Frontages. All development is subject to frontage standards and guidelines (Section 4.7). Ground-floor corner units must have two active frontages, but are not required to provide an entrance for each.
- E. Private Open Space. All development is subject to open space and landscape standards and guidelines (Section 4.8, Appendix A).
- F. Articulation. Deep, well-proportioned, and detailed openings with high-quality windows are encouraged. Windows should be recessed and sun-shaded except on north facing facades which may be sheer.

² For the purposes of this Code, "redevelopment" shall include any project that either demolishes 50 percent or more of the existing floor area of the primary building on site, or adds an amount of floor area that is equal to or more than 50 percent of the existing floor area of the primary building on site.

- G. Height. Height limits are based on District. Hotels and the hospital are not subject to height limits but are not expected to exceed six stories.
- H. Vehicular parking. Parking requirements are at the discretion of the approving body. Applicants are required to demonstrate to the satisfaction of the approving body that the development provides a sufficient amount of on-site parking, taking into account context, use, hours of activity, and participation in shared-parking agreements.
- I. Rear access. Alleys are required for any block containing at least one lot less than 60 feet wide and are required to be privately maintained.
- J. Floor plans. In ground-floor dwelling units, all private rooms (bathrooms and bedrooms) should be grouped at the rear of the building, while all social rooms (kitchen, dining, living, etc.) should be grouped at the front of the building.
- K. Clutter. All utilities and above-ground equipment shall be located in the rear of the lot, preferably in the alley. Rooftop equipment screening shall be an integral element of building architecture.
- L. Fences & Walls. Fences and walls may not exceed an average of 3.5 feet in height, calculated at intervals no less than 2 feet in length, from lowest adjacent grade within any front yard setback, street side yard setback, or within 5 feet of any front or street side façade, and may not exceed 4 feet in height within these areas. Fences and wall may reach up to 6 feet from adjacent grade anywhere else on the property.

**Table 4.2
Summary of Development Standards by District**

	ZONES					
	DC		DG		DE	
Block Size						
Commercial/Mixed Use						
Depth	470'					
Perimeter	1,600'					
All Residential						
Depth	300'					
Perimeter	1,200'					
Lot Size						
Width	25'	---	25'	---	25'	4.4.4.D
Depth	90'	300'	90'	220'	80'	160'
Building Placement						
On-site Open Space (% of lot area)	See Section 4.8		See Section 4.8		10%	
Amount of façade at front setback line	80%		65%		60%	
Amount of façade at side street setback line	60%		50%		60%	
Rear Setback (with alley)	15' from alley centerline					
Rear Setback (without alley)	10'		10'		15'	
Ground Floor Commercial (min)						
Front Setback	0' or 10' ¹		5'		10'	
Street Side Setback	0' or 5'		5'		5'	
Interior Side Yard Setback	0' or 10' ²		0' or 10' ²		0' or 10' ²	
Ground Floor Residential (min)						
Front Setback	10'		10'		15'-20'	
Street Side Setback	5'		5'		10'	
Interior Side Yard Setback	N/A		5'-10'		5'-10'	
Building Height						
To highest top place (max)	75', 6 stories ³		50', 4 stories		30', 3 stories	
Ground story height floor to floor	15' min		12' min		9' min	

**Table 4.2 (cont.)
Summary of Development Standards by District**

	ZONES		
	DC	DG	DE
Parking & Vehicular Access			
All new parking facilities or modifications to existing parking facilities must comply with the following standards and the additional standards in Section 4.6, Parking Areas and Facilities.			
Parking Setbacks			
Primary Street	30'	30'	Rear 1/3 of lot
Side Street, within 75' of front property line	30'	30'	---
Side Street, 75' back from property line	5' ⁵	5' ⁵	---
Side Yard	0'	0'	---
Rear (with or without alley)	5'	5'	---
On-Site Vehicular Access			
Side Street Drive (not within 75' of front façade)	12' 1-way; 24' 2-way		N/A
Front Drive (only within 25' of interior lot lines)	12' 1-way; 24' 2-way		N/A
Alley access shall be provided via a rear service lane for all lots in the DE zone, and all lots less than 60 feet wide.			
Notes:			
<ol style="list-style-type: none"> 1. Shopfronts must meet the front property line, or otherwise utilize a 10' front setback for programmed area, such as outdoor dining, seating, retail or art displays, etc. 2. If setback is provided, it must be in the form of a paseo (see Section 4.11.4C and 4.10.11) towards which secondary shopfronts may open. 3. Hospitals and Hotels are exempt. 4. Fully-subterranean garages may extend to rear and side-yard lot lines. Semi-subterranean garages are subject to building setbacks. 5. When parking is located within building, the building must respect this setback. 			

4.4.2 Specific to District Core



Intent and Physical Character

The District Core is characterized by active, pedestrian-oriented medical, retail, restaurant, art galleries, and other ground-floor uses set at or near the sidewalk, with market-rate housing and offices on upper floors. Buildings are up to 6 stories, except hotels and hospitals, which are not subject to height limits.

Standards

These standards are required to generate and maintain the DC zone's physical character and range of uses.

1. Site Design, Building or addition(s).
 - a. Maximum block depth/perimeter. See Table 4.2.
 - b. New Lot Size

	Min	Max
Width	25'	--
Depth	90'	300'

- c. Building Setbacks/Placement: Setbacks apply to all stories unless stated otherwise.

	Ground Floor, Non-Residential
Primary street	0' to 10' ¹
Side street	0' to 5'
Interior Side Yard	0' to 10' ²
Rear with Alley Access	15' from alley centerline
Rear without Alley	Min 10' from rear property line
Amount of façade at primary street setback line (% of lot width)	Min 80%
Amount of façade at side street setback line (% of lot width)	Min 60%
Notes:	
<ol style="list-style-type: none"> 1. Shopfronts must meet the front property line, or otherwise utilize a 10' front setback for programmed area, such as outdoor dining, seating, retail or art displays, etc. 2. If setback is provided, it must be in the form of a paseo towards which secondary shopfronts may open (see Sections 4.11.4C and 4.10.11) 	

- d. Ground Floor: At least 15 feet tall in compliance with the frontage requirements in Section 4.7.
- e. Building Height:

To highest top plate ¹	75'; 6 stories
Ground story height floor to floor	15' min
Ground story depth	30' min
Notes:	
<ol style="list-style-type: none"> 1. Hotels and Hospitals are exempt from height limits. 	

- f. Massing Requirements:

Horizontal Massing: Non-residential buildings that have street-facing facades greater than 100 feet in width must visually differentiate themselves into multiple sections, each being 100 feet wide or less. Residential buildings that have street-facing facades greater than 75 feet in width must visually differentiate themselves into multiple sections, each being 75 feet wide or less.

The goal is for large buildings to look like a collection of smaller ones. This can be achieved by organizing ground-floor bays and the windows stacked above them into groupings that reinforce a narrower lotting pattern, and applying different height and material(s) to each. Additionally, horizontal building features should be off-set from one another, and window shape, size, and pattern should differentiate from façade to façade. The most effective way to achieve convincing results is for a different designer or architect to independently design each façade. Ground-level or above-ground courts, and/or mid-block paseos can also be effective.

Vertical Massing: Buildings should be vertically organized into three parts: a strong base, a defined middle, and a cornice.

2. Visitor/Pedestrian Access.

Applicable to new buildings, modification of street-facing facades or new upper floor(s).

- a. Building Access. Access to each building and ground floor space(s) is required from the sidewalk, paseo, or court via the allowed frontage in Section 4.7. The maximum distance between the public right-of-way and the primary entrance to the building or ground floor unit is 100 feet.
 - b. Upper Floor Access. Access to upper floor dwellings and commercial spaces is required from the street via shared court, paseos, or a lobby.
 - c. Side Street Facades. These facades are to be designed to the same level of architectural treatment and materials as the front façade.
3. Frontages. Applicable to the modification of or construction of new street-facing facades. See Section 4.7.
4. Signage. See Section 4.9.
5. Private Open Space. Applicable to new buildings and additions over 50%. See Section 4.8
6. Uses. Applicable to new uses. See Section 4.5.

7. Vehicular Access and Parking. Applicable to modification of or new curb cuts, driveways, and parking areas. See Section 4.6.

Parking Placement	
(min setbacks are measures from each lot line)	
All new parking facilities or modifications to existing parking facilities must comply with the following standards and the additional standards in Section 4.6, Parking Areas and Facilities.	
Setbacks¹	
Primary Street	30'
Side Street, within 75' of front property line	30'
Side Street, 75' back from front property line	5' ²
Side Street	0'
Rear (with or without alley)	5' ²
On-Site Vehicular Access (min)³	
Side Street Drive (not within 75' of front façade)	12' 1-way
Front Drive (only within 25' of interior lot line)	24' 2-way
Alley access (vehicular access shall be provided via a rear service lane for all lots less than 60 feet wide)	
Notes:	
<ol style="list-style-type: none"> 1. Fully subterranean garages may extend to rear and side-yard lot lines. Semi-subterranean garages are subject to building setbacks. 2. When parking is located within building, the building must respect this setback. 3. Exceptions to the access requirements may be possible on a project-by-project basis with approval of the City's Traffic Engineer. 	

4.4.3 Specific to District General



The District General Zone consists of 2-4 story buildings, both residential and nonresidential.

Intent and Physical Character

The District General is characterized by a mix of non-residential ground floor frontages with shopfronts and residential ground floors set back behind pedestrian-oriented frontages and resident/visitor access via dooryards, stoops, and spacious lobbies.

Standards

These standards are required to generate the intended character of the DG zone.

1. Site Design, Building or addition(s).

Applicable to a new lot, new building, or addition(s).

- a. Maximum block depth/perimeter. See Section 4.3.
- b. New Lot Size:

	Min	Max
Width	25'	Block Face
Depth	90'	220'

- c. Building Setbacks: Setbacks apply to all stories unless stated otherwise.

	Ground Floor	
	Non-Residential	Residential
Primary street	0' to 10' ¹	Min 10'
Side street	0' to 5'	Min 10'
Interior Side Yard	0' to 10' ²	5' to 10'
Rear with Alley Access	15' from alley centerline	
Rear without Alley	Min 10' from rear property line	
Amount of façade at primary street setback line (% of lot width)	Min 65%	Min 65%
Amount of façade at side street setback line (% of lot width)	Min 50%	Min 50%
Notes:		
1. Shopfronts must meet the front property line, or otherwise utilize a 10' front setback for programmed area, such as outdoor dining, seating, retail or art displays, etc.		
2. If setback is provided, it must be in the form of a paseo towards which secondary shopfronts may open (see Sections 4.11.4C and 4.10.11)		

- d. Ground Floor: At least 12 feet tall in compliance with the frontage requirements in Section 4.7.

- e. Building Height:

To highest top plate ¹	50'; 4 stories
Ground story height floor to floor	12' min
Ground story depth	30' min

- f. Massing Requirements:

Horizontal Massing: Non-residential buildings that have street-facing facades greater than 75 feet in width must visually differentiate themselves into multiple sections, each being 75 feet wide or less. Residential buildings that have street-facing facades greater than 50 feet in width must visually differentiate themselves into multiple sections, each being 50 feet wide or less.

The goal is for large buildings to look like a collection of smaller ones. This can be achieved by organizing ground-floor bays and the windows stacked above them into groupings that reinforce a narrower lotting pattern, and

applying different height and material(s) to each. Additionally, horizontal building features should be off-set from one another, and window shape, size, and pattern should differentiate from façade to façade. The most effective way to achieve convincing results is for a different designer or architect to independently design each façade. Ground-level or above-ground courts, and/or mid-block paseos can also be effective.

Vertical Massing: Buildings should be vertically organized into three parts: a strong base, a defined middle, and a cornice.

2. Visitor/Pedestrian Access.

Applicable to new buildings, modification of street-facing facades or new upper floor(s).

- a. Building Access. Access to each building and ground floor space(s) is required from the sidewalk, paseo, or court via the allowed frontages in Section 4.7. The maximum distance between the public right-of-way and the primary entrance to the building or ground floor unit is 100 feet.
 - b. Upper Floor Access. Access to upper floor dwellings and commercial spaces is required from the street via shared court, paseos, or a lobby.
 - c. Side Street Facades. These facades are to be designed to the same level of architectural treatment and materials as the front façade.
3. Frontages. Applicable to the modification of or construction of new street-facing facades. See Section 4.7.
4. Signage. See Section 4.9.
5. Private Open Space. Applicable to new buildings and additions over 50%. See Section 4.8.
6. Uses. Applicable to new uses. See Section 4.5.
7. Vehicular Access and Parking. Applicable to modification of or new curb cuts, driveways, and parking areas. See Section 4.6

Parking Placement	
(min setbacks are measures from each lot line)	
All new parking facilities or modifications to existing parking facilities are required to be in compliance with the following standards and the additional standards in Section 4.6, Parking Areas and Facilities.	
Setbacks¹	
Primary Street	30'
Side Street, within 75' of front property line	30'
Side Street, 75' back from front property line	5' ²
Side Street	5'
Rear (with or without alley)	5' ²
On-Site Vehicular Access (feet, maximum)³	
Side Street Drive (not within 75' of front façade and when alley not present)	12' 1-way
Front Drive (only within 25' of interior lot line)	24' 2-way
Alley access (vehicular access shall be provided via a rear service lane for all lots less than 60 feet wide)	
Notes:	
<ol style="list-style-type: none"> 1. Fully subterranean garages may extend to rear and side-yard lot lines. Semi-subterranean garages are subject to building setbacks. 2. When parking is located within building, the building must respect this setback. 3. Exceptions to the access requirements may be possible on a project-by-project basis with approval of the City's Traffic Engineer. 	

4.4.4 Specific to District Edge



The District Edge consists of narrow to medium, attached and detached house form buildings.

Intent and Physical Character

The District Edge is characterized by a mix of housing types with residential ground floors setback behind pedestrian-oriented frontages with resident and visitor access via dooryards, stoops, and porches. Buildings are up to 3 stories in height. Some neighborhood serving commercial is allowed.

Standards

These standards are required to generate and maintain the DE zone’s physical character and range of uses.

1. Site Design, Building or addition(s).

Applicable to a new lot, new building, or addition(s).

- a. Maximum block depth/perimeter. See Section 4.3.
- b. New Lot Size:

	Min	Max
Width	25'	Per 4.4.4D
Depth	80'	160'

c. Building Setbacks: Setbacks apply to all stories unless stated otherwise.

	Ground Floor	
	Non-Residential	Residential
Primary street	Min 10'	15' to 20'
Side street	Min 5'	Min 10'
Interior Side Yard	0' to 10'	5' to 10'
Rear with Alley Access	15' from alley centerline	
Rear without Alley	N/A	N/A
Amount of façade at primary street setback line (% of lot width)	Min 60%	
Amount of façade at side street setback line (% of lot width)	Min 50%	
On-site Open Space (% of lot area)	N/A	Min 10%

d. Ground Floor: At least 10 feet tall in compliance with the frontage requirements in Section 4.7.

e. Building Height:

To highest top plate ¹	30'; 3 stories
Ground story height floor to floor	9' min
From top plate to roof peak	7.5' max

f. Massing Requirements: Buildings in the District Edge will take the form of either single house-form units or attached house-form units.

DETACHED	Max
Lot Width	60'
Overall Building Dimension	50'
Primary mass width/depth	40'/50'
Secondary mass width or depth	25'
Buildings allowed per lot	1 primary, 2 outbuildings
Multi-unit house form buildings are allowed.	

ATTACHED (ROWHOUSES, LIVE/WORK)	
Lot Width	200' max.
Unit Width	22' min.
Units in Array	2-8
Building Depth	30' to 45'
Outbuildings	1 per unit

- g. Unit Orientation. Ground floor units adjacent to the street must orient to the street, others may face court, paseo, or yard, but not a parking lot, access drive, or alley.

2. Visitor/Pedestrian Access.

Applicable to new buildings, modification of street-facing facades or new upper floor(s).

- a. Building Access. Access to each building and ground floor space(s) is required from the sidewalk, paseo, or court via the allowed frontages in Section 4.7. The maximum distance between the sidewalk and the primary entrance to a building or ground floor unit is 100 feet.
 - b. Upper Floor Access. Access to upper floor dwellings is required from the street via shared court, paseos, or a lobby.
 - c. Side Street Facades. These facades are to be designed to the same level of architectural treatment and materials as the front façade.
3. Frontages. Applicable to the modification of or construction of new street-facing facades. See Section 4.7.
4. Signage. See Section 4.9.
5. Private Open Space. Applicable to new buildings and additions over 50%. See Section 4.8.
6. Uses. Applicable to new uses. See Section 4.5.
7. Vehicular Access and Parking. Applicable to modification of or new curb cuts, driveways, and parking areas. Section 4.6.

Per direction of the City Traffic Engineer, parking may only be provided in the rear third of a lot. For all lots in the DE zone, on-site parking may only be accessed via alley. Building placement standards apply to detached garages/carports. Subterranean parking is exempt from this standard.

4.5 Allowed Uses

4.5.1 Applicability

This section identifies the allowed uses and corresponding permit requirements within each district. All uses are subject to any applicable development standards, design guidelines, State law, and any other applicable requirements that are beyond the scope of this Code. Descriptions/definitions of the uses can be found in LMC Section 17.04.240, Definitions, and Appendix B of this Code. In cases where a term is defined in both LMC Section 17.04.240 and Appendix B of this Code, the definition of this Code shall prevail. If a term used in the Health District Code is not defined in LMC Section 17.04.240 or Appendix B of this Code, the Director shall determine the definition, giving deference to common usage. Uses that are not listed are not allowed unless the Director makes a similar use determination. However, the Director has the final determination in all cases.

**Table 4.3
Allowed Uses**

Uses	Districts		
	Edge	General	Core
Residential			
Dwelling Unit	P	P	D
Lodging			
Hotel (no room limit)	--	D	P
Inn (up to 12 rooms)	--	D	--
Bed & Breakfast (up to 5 rooms)	P	--	--
Automotive			
Gas Station	--	--	--
Drive-through	--	--	--
Mini-marts/convenience stores	--	--	--
Service	--	--	--
Office			
Non-Medical Office/Studio	D	P	C
Civic			
Library	--	P	P
Theater ≥ 10K square feet	--	C	C
Theater < 10K square feet	D	D	P
Assembly	C	C	C
Museum/Gallery	P	P	P
Fire/Police Station	--	P	C

**Table 4.3
Allowed Uses (cont.)**

Uses	Districts		
	Edge	General	Core
Education			
Elementary School	D	D	--
Day Care Center	D	D	D
Studio	C	P	P
Retail/Service			
Restaurant with alcohol	D/C ¹	D	D
Restaurant without alcohol	D/C ¹	P	P
Health Club	--	D	D
Grocery Store/Market	C	D	D
Bar	--	C	C
Bank	--	C	C
Personal Services	D	P	P
General Retail	--	P	P
Liquor Store	--	--	--
Medical			
Hospital	--	--	P
Medical Office	--	P	D
Veterinary Clinic, Small Animal	--	P	D
Industrial			
Artisanal Production	--	P	D
Labs, Research & Development	--	P	D
Notes: P – Permitted; C – Conditional Use Permit; D – Director’s Review; “--” not allowed			
1. Level of review to be determined by the Development Services Director or his designee on a case-by-case basis.			

4.6 Parking Areas & Facilities

4.6.1 Applicability

The standards of this section apply to all new surface parking lots and structured parking.

4.6.2 Surface Parking

- A. Intent. The intent of this section is to provide guidelines for parking areas so that they are comfortable, as well as appropriately located and configured so as not to diminish the quality of the public realm.
- B. Access
 - 1. Vehicular Access. Driveways that cut across sidewalks may not affect the slope or direction of the sidewalk, and turning radii must be as tight as practical. Deviations may be approved by the City's Traffic Engineer on a case-by-case basis to ensure pedestrian and traffic safety.
 - 2. Pedestrian Access. All pedestrian access shall be intuitive and direct, well-lit, and separate from vehicular right-of-ways.
- C. Configuration. A variety of surface parking configurations are available to use, including head in, diagonal, one-way or two-way. The configuration that provides the most efficient parking layout for a given space shall be employed.
- D. Screening. Surface parking spaces may be open or covered. All surface parking should be screened from street views by buildings, walls (36 to 48 inches tall) or the following screening strategies.
 - 1. Landscape screening. Trees and shrubs are selected for their ultimate scale to the space.
 - 2. Screening Structures/ Facades. Screening devices may include various elements such as walls, perforated metal panels, wire panels, finished concrete and other high-quality materials that maintain architectural sensitivity to the surrounding buildings and character.
 - 3. Public Art/Murals. In accordance with the City's public art program, surface lots may incorporate public art elements such as sculptures, mural paintings, images and other artistic façade treatments and installations.
- E. Shade. Shade should be provided throughout surface lots. In addition to landscape elements, a minimum 30 percent of the parking area should be shaded by trees or through the following strategies:

1. Shade Structures. Structures may include arbors, trellises, pergolas, mesh and overhead canopies.
2. Solar Shade Structures. For lots without public frontage and not visible from the sidewalk, standalone solar structures may be used. For lots within public view, individual solar panels may be applied to existing shade structures.

F. Lighting

1. Outdoor light fixtures shall not exceed 15 feet in height.
2. Lighting shall be recessed and shielded so that:
 - i. The light source (e.g., bulb, etc.) is not visible from the project site; and
 - ii. Glare and reflections are confined within the boundaries of the project site. Each light fixture shall be shielded and directed downward and away from adjoining properties and the public right-of-way.
3. Illumination levels should be minimum 1 foot-candle and no more than 7.

G. Landscape. Drought tolerant plant materials should be applied in compliance with the following:

1. Location. Landscaping should be evenly dispersed in a parking area with trees planted around the perimeter. For larger parking areas, orchard-style tree plantings (placed in uniformly-spaced rows) are recommended.
2. Irrigation. Appropriate irrigation and drainage shall be provided for all landscaped areas.

H. Paving. Parking areas should be designed to reduce the amount of runoff-generating surface area. The following permeable surfaces for parking and maneuvering areas are encouraged subject to fire department approval.

1. Pervious asphalt and concrete
2. Permeable pavers
3. Reinforced grass paving

I. Finishes. Parking lot and structure materials, finishes, fixtures, and colors should be designed in a manner that is consistent with the architectural character of adjacent buildings.

J. Amenities. The following amenities may be integrated to support alternative modes of transportation and sustainability. These amenities should be located in convenient locations to incentivize their use.

1. Bicycle Parking. Bicycle parking may be located within surface lots.

2. Clean Air/Electric Vehicle Parking and Charging Stations. Parking facilities may incentivize the use of clean air and electric vehicles.
3. Motorcycle/Scooter Parking. Parking for motorcycles and scooters may be provided in compliance with city standards.



This is a more welcome pedestrian environment than many surface parking lots. Permeable paving helps reduce urban runoff, and generous tree-shading helps reduce urban heat.

4.6.3 Structured Parking

- A. Intent. The intent of this section is to provide guidelines for structured parking so that they are comfortable and appropriately located and configured so as not to diminish the quality of the public realm.
- B. Access, Configuration and Size
 - 1. Access
 - a. Vehicular Access. Driveways that cut across sidewalks may not affect the slope or direction of the sidewalk, and turning radii must be as tight as practical. Deviations may be approved by the City's Traffic Engineer on a case-by-case basis to ensure pedestrian and traffic safety.
 - b. Pedestrian Access. All pedestrian access shall be intuitive and direct, well-lit, and separate from vehicular right-of-ways.
 - 1. Pedestrian bridges. Direct pedestrian access from upper levels of parking structures to residential buildings is allowed except across a private or public street (alleys okay).
 - 2. Configuration
 - a. Lining at front property line. All parking must be behind at least 30 feet of occupiable space, which should be designed per the Frontage & Access Standards in Section 4.7.
 - b. Lining at side property line (corner lots). All parking must be behind at least 30 feet of occupiable, useable space for the first 75 feet, as measured from the front property line. The ground level for this stretch is required to comply with the Frontage Standards in Section 4.7. Non-lined portions of above-ground garages must be set back at least five feet. The setback area should contain landscaping of the same quality as elsewhere on the property.
 - c. Subterranean. Fully-subterranean garages may extend to rear and side-yard lot lines. Semi-subterranean garages are subject to building setbacks.
- C. Design. Non-lined portions of parking structures should not be of an aesthetic quality inferior to the rest of the building. Rather, each should be designed as a

continuation of the building it serves, with the same level of detail, pattern of openings, and should look as if not a garage.



The non-lined portion of structures should be treated with the same quality of design as the rest of the building. Frequent and vertically-oriented windows, façade differentiation, and traditional architectural elements all contribute to this.



The non-lined portion of structures should be set back five feet from the side property line, and well-landscaped.

4.7 Urban Frontage Standards

4.7.1 Intent

The Health District is to be a world-class, active, pedestrian-oriented urban environment that is comfortable, safe, and easy to navigate through by foot, bicycle, or any mode of vehicular transit. As such, careful attention must be given to the way that buildings shape and front that environment, also known as the “Public Realm.” The space between ground-floor building face(s) and the public street or open space it fronts is defined in this code as the “Urban Frontage,” and the calibrated design of that space to the ground-floor use of the adjacent buildings and the unified district character will be critically important to creating the high-quality urban environment described in Chapter 3 of this Plan.

4.7.2 Applicability

Each new building, façade renovation, new street and street retrofit/renovation is to be designed in compliance with this section. As illustrated in Figure 4.7.4A, the “Urban Frontage” refers to space between a building’s façade/entry and street or open space it fronts.

When buildings front a public or semi-public open space, the frontage extends to the edge of the open space. This Code differentiates between private and public frontage, the former extending from building face to right-of-way line, and the latter extending from the right-of-way line to the first travel lane (vehicular or bicycle) (Figure 4.7.4A). This Section contains standards and guidelines specific to commercial ground floors (Section 4.7.5) and residential ground floors (Section 4.7.6). Additional standards and guidelines can be found in Table 4.4, Development Standards; Section 4.8, Access & On-Site Open Space; and Section 4.11, Public Open Space.

4.7.3 Urban Frontage Components

As illustrated in Figures 4.2 and 4.3, Urban Frontages, particularly those in a very urban environment such as is envisioned for the Health District, are made up of several layers. Each layer has a specific function, and it is the collective design of these layers, carefully tied to the ground-floor use and district character, that will ultimately determine whether or not the Health District is an active, attractive, comfortable, and safe urban environment with lasting value that will attract new long-term investment.

Collectively, these layers make up seams between the Public Realm (streets, paseos, and public open spaces) and Private Realm (private units within buildings); accordingly, all Urban Frontages are organized into two categories:

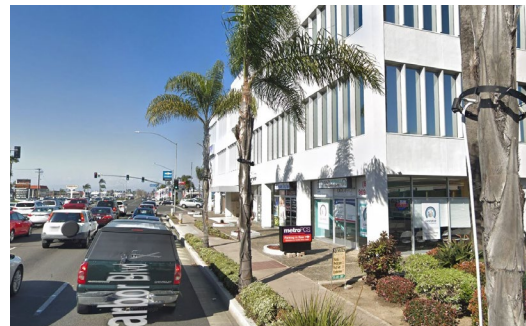


Well designed Urban Frontages encourage activity, promote interaction, and activate streets; essential qualities of the Health District.

- A. Public Frontages. Within the public right-of-way; the space between the back of the sidewalk to the first travel lane (vehicular or bike) of the public street; generally organized into the following zones, further described in Section 4.7.4:
 - a. Pedestrian zone
 - b. Landscape, Furniture & Street Lighting zone
 - c. Flex Curbside zone
 - d. Café zone (often part of Private Frontage)
- B. Private Frontages. Outside of the public right-of-way; typically the space between the back of the sidewalk and the primary building or ground-floor unit entry, further described in Section 4.7.4.



Urban frontage in a main street environment with clearly articulated curbside, landscape, pedestrian, and café zones.



AVOID: Disorganized Urban Frontage, lacking articulated zones, and poorly buffered from street traffic.

4.7.4 Urban Frontage Design Standards

The following design standards, organized by Public and Private Frontages and their variations, apply to all Urban Frontages in the Health District.

Public Frontages

Public Frontages are made up of the following three zones:

1. **Pedestrian Zone.** The sidewalk, or portion of it, that is dedicated to pedestrian circulation only and free and clear of any obstructions (street furniture, landscape planters, bike parking, driveway ramps). Table 4.4 provides additional design standards for this zone.
2. **Landscape, Furniture & Lighting Zone.** This zone extends from the outer edge of sidewalk (pedestrian zone) to the face of curb, and is a dedicated space for streetscape furnishings and landscaping that provides shade to pedestrian and parked cars, and a buffer between pedestrians and street traffic. The design of this space is tied to the predominant ground-floor use of the fronting buildings:
 - i. **Commercial Frontages** (see Figure 4.2). This zone should be mostly hardscaped, and well-furnished with benches, bicycle racks, tree wells, street lighting, and other similar amenities. Pervious paving materials are encouraged.
 - ii. **Residential Frontages** (See Figure 4.3). This zone should be mostly landscaped via continuous parkways, with occasional hardscape breaks for pedestrian crossings. (See Appendix A: Landscaping Guidelines). Table 4.4 provides additional design standards for this zone.
3. **Flex Curbside Zone.** This zone extends from the street curb to the first travel lane (vehicular or bicycle), and is purposefully flexible in use and programming based on the evolving needs of the district, including curbside parking, pedestrian pick-up and drop-off, street landscaping, stormwater management, and a variety of others. Curbside services such as valet, passenger loading/unloading, and commercial deliveries require the review and approval of the Traffic Section. Table 4.4 provides additional design standards for this zone.
4. **Café Zone.** This zone is only applicable in commercial/retail environments where a portion of the sidewalk is dedicated to outdoor dining or merchandising. This can be part of the public right-of-way or part of the Private Frontage, by way of a recessed or setback storefront. Table 4.4 provides additional design standards for this zone and Section 4.7.5 provides design standards for a variety of commercial design variations of this zone.

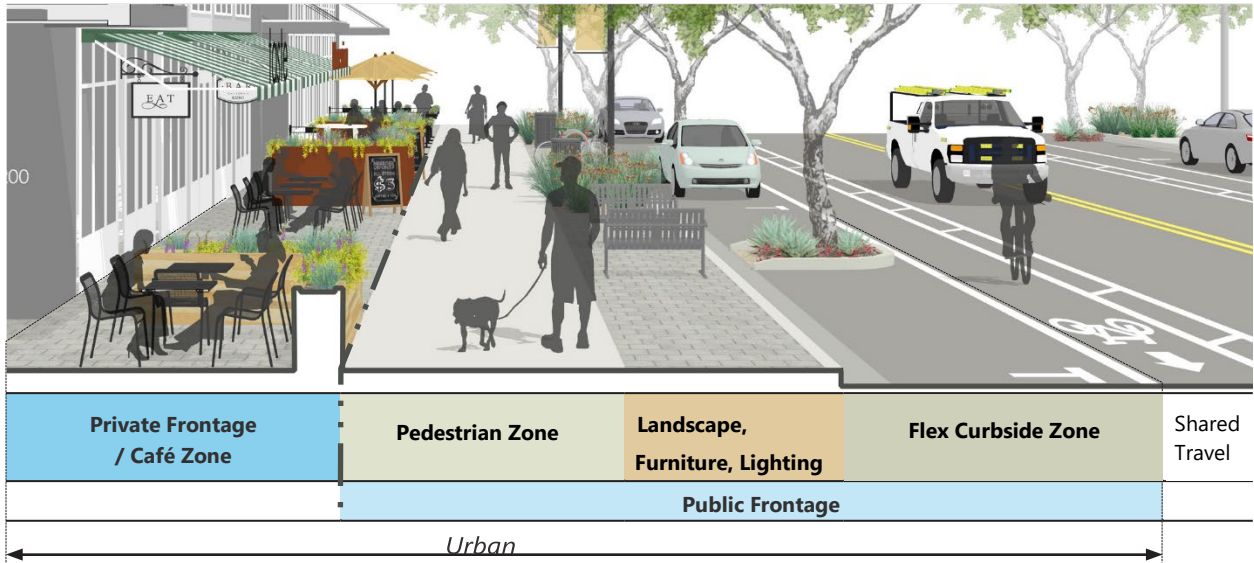


Figure 4.2, Urban Frontage Components – Commercial/Mixed Use

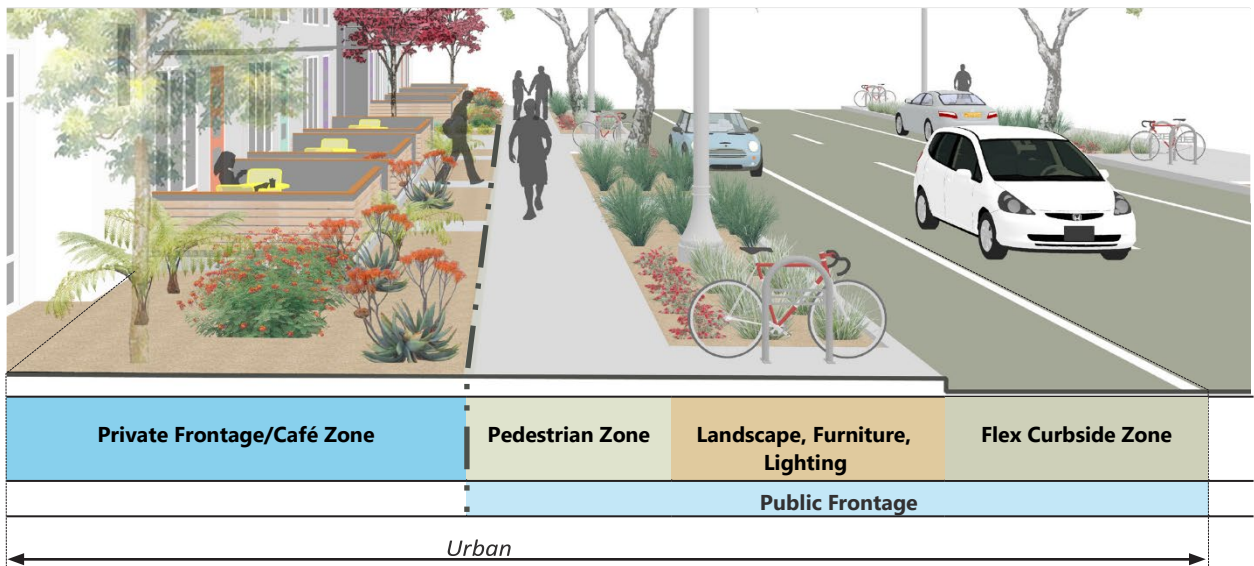


Figure 4.3, Urban Frontage Components – Residential Environments

**Table 4.4
Public Frontage Standards**

Public Frontage	Minimum Dimensions by District				Allowed Elements
	DC	DG	DE	OS ¹	
Pedestrian Zone					
Commercial	10'	8'	6'	Per zone	Must be free of any/all obstructions, including furnishings, landscaping, driveway ramps.
Residential	8'	6'	5'	Per zone	
Landscape, Furniture & Lighting Zone	8'	6'	6'	Per zone	Street furniture, street trees & landscaping, street lighting, district branding & wayfinding signage, bike/scooter parking.
Flex Curbside Zone	7'	7'	7'	n/a	Short-term parking, passenger loading zones, car-share depots, bike-share/parking, stormwater management, parklets
Café Zone	8'	5'	5'	Per zone	Outdoor dining, outdoor merchandising, approved sidewalk signs.
Notes:					
1. Public open spaces providing frontage/access to buildings must satisfy the requirements of Table 4.7.4A based on the regulatory zone the open space is within. All public open spaces are also subject to Public Open Space Standards in Section 4.11.					

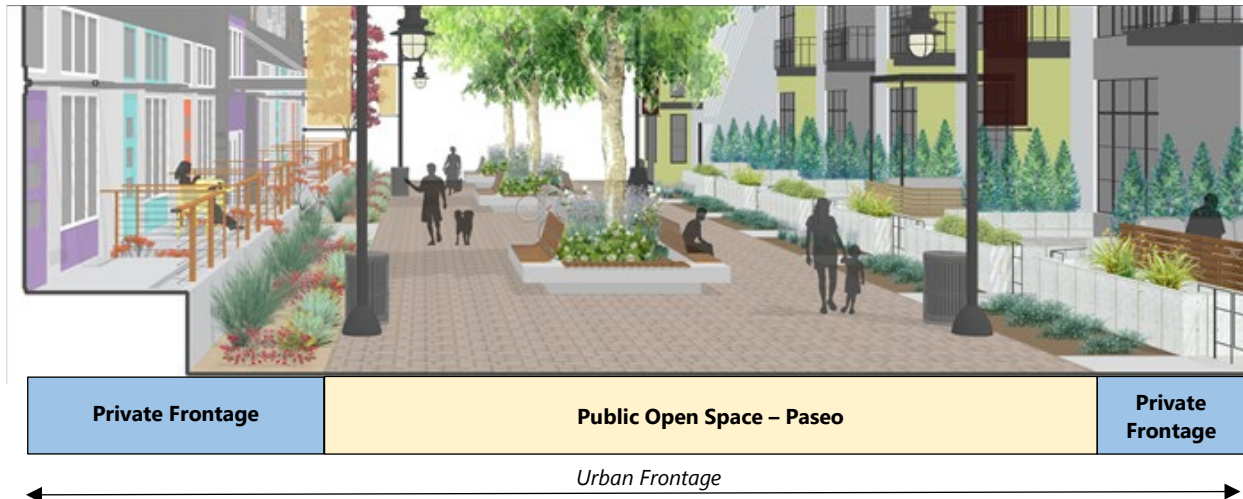


Figure 4.4, Urban Frontage Components – Open Space

Private Frontages

Private frontages are elements of buildings that function as the seam between the public realm (streets and public open spaces) and private realm (interior units of buildings). The careful design of each building’s private frontage is essential for generating the active, vibrant public realm envisioned for the Lancaster Health District. The following standards apply to all private frontages:

1. Calibration to Ground Floor Use. Private frontages must be designed to provide an appropriate degree of privacy calibrated to the use of the public-facing ground-floor units of buildings; with commercial shops and spaces relying on more exposure for their success, and ground-floor residences needing added privacy to encourage “eyes on the street”. Section 4.7.5 and Section 4.7.6 provide design standards specific to Commercial and Residential Frontages, organized by access types.
2. Access. All street-adjacent ground-floor units and all upper-floor units must take primary access directly from the sidewalk. Each building must have an entrance for every 50 feet of frontage (even spacing not required) along all facades within an overlay or on any north/south street. One entrance is required per building on non-overlay east/west facades (entrances on chamfered corners satisfy this requirement). There are two fundamental frontage/access types, within which, many variations are possible:

- a. Direct entry. Access to each ground floor unit is provide via a direct entry onto the public realm, or common yard/court that may be accessed from the public realm;
 - b. Common entry. Entry to the building is provided via a single common entry and lobby, and access to individual units (ground and upper floors) is provided via a shared hall/corridor within the building.
3. Corner Lots. Buildings on corner lots (lots fronting/siding onto two or more public streets, or public open spaces) shall provide active frontages on all public sides.
4. Fenestration. All facades fronting or siding on a street or public open space must be articulated by frequent openings.
 - a. Design. All individual windows must be recessed and well-framed.
 - b. Frequency. The horizontal distance between ground-floor windows or entrances shall not exceed 10 feet.
 - c. Transparency. Ground floor windows must have a transparency higher than 80% and an external reflectance of less than 15%. For nonresidential space, all shade should be provided on the exterior of the building; the majority of the interior space should be visible from the right-of-way.
 - d. Façade coverage. The following minimum areas must consist of windows, doors (where applicable), and their framing elements. The ground floor is the entire horizontal area from floor to ceiling; the “upper floors” is the entire horizontal area from the floor of the second floor to the ceiling of the top floor.
 - i. Ground-floor residential units – 40%. Sills of windows contributing to this minimum may be no higher than 5 feet from adjacent grade.
 - ii. Ground floor nonresidential space – 60%. Sills of windows contributing to his minimum may be no higher than 3 feet from adjacent grade.
 - iii. Upper floors (any use) – 25%
5. Fences and Walls.
 - a. General. Fences and walls may not exceed an average of 3.5 feet in height, calculated at intervals no less than 2 feet in length, from lowest adjacent grade within any front yard setback, street side yard setback, or within 5 feet of any front or street side façade, and may not exceed 4 feet in height within these areas. Fences and walls may reach up to 6 feet from adjacent grade anywhere else on the property.

- b. Specific to courtyard buildings. Courtyard buildings with ground-floor commercial uses may be enclosed during non-business hours by an open-work gate not exceeding 8 feet in height.
6. Grading. Grading between the façade and front property line must result in an average slope between 0.5% and 5%.
7. Frontage Elements. The following pages contain an array of frontage elements that are appropriate for the downtown area. In addition to the standards above, the standards specific to each frontage element apply wherever such element occurs. As shown in Table 4.5, the permitted location of each element is determined by district and use.

**Table 4.5
Private Frontage Standards**

Private Frontage Types	Allowed Per Zone			Allowed Encroachment into Setbacks	Notes/Section References
	DC	DG	DE		
DIRECT ENTRY					
Commercial Ground Floors					
Shopfront	●	●	●	Awning w/in 2' of curb	Section 4.7.5B
Commercial Terrace	●	●	--	N/A	Figure 4.7.5C-1
Commercial Court	●	●	--	N/A	Figure 4.7.5C-2
Gallery/Arcade	●	●	●	Within 2' of curb	Figures 4.7.5C-3 and 4.7.5C-4
Residential Ground Floors					
Dooryard	●	●	●	Within 2' of sidewalk	Section 4.7.6B
Porch	--	--	●	Within 5' of sidewalk	Figure 4.7.6C-1
Stoop	--	●	●	Within 2' of sidewalk	Figure 4.7.6C-2
Residential Terrace	●	●	●	Within 2' of sidewalk	Figure 4.7.6C-3
Residential Court	●	●	●	N/A	Figure 4.7.6C-4
COMMON ENTRY					
Commercial/Office	●	●	--	Awning/Canopy	Section 4.7.5
Residential/Mixed-Use	●	●	●	Within 2' of curb	Section 4.7.6
Notes: ● : Allowed --: Not Allowed N/A: Not applicable					



Direct Entry (residential): Ground-floor units are accessed directly from the sidewalk via stoops.

Common Entry (commercial): Common entry and lobby, activated by a transparent ground floor.



4.7.5 Specific to Commercial Frontages

Common Entrance

The Common Entrance provides access to a lobby and/or corridor that services multiple units.

1. Lobbies shall be visible from the right-of-way.
2. Common entrances should be conspicuous and designed as a focal-point of the ground-floor façade.
3. Designs which engage multiple floors are encouraged.
4. Designs which help modulate the façade of the main building are encouraged.
5. Common entrances should provide a comfortable attractive outdoor public transitional space that allows congregation off of the sidewalk. This area of outdoor space should be designed with pedestrian amenities, such as benches or planters.
6. Outdoor spaces may be elevated or sunken, accessed by stairs and ramps, and may additionally be enclosed by landscaping. See Section 4.8: Access & On-Site Open Space.
7. Common entrances may be combined with frontage elements:
 - a. Commercial Terrace
 - b. Commercial Court
 - c. Arcade/Gallery

Table 4.6
Commercial Common Entry

Frontage Element	Min	Max
Height to top of transom	10'	20'
Height to bottom of canopy/awning	8'	-
Distance to back of sidewalk	-	20'
Area of outdoor space ¹	100 sf	-
Ground floor fenestration (clear)	70%	-
Awning/Canopy Depth	4'	-
1. Does not include public right of way.		



Direct Entrance

A Direct Entrance to a commercial ground floor is in the form of a shopfront. The basic form of a shopfront is a large opening in the façade at or near the sidewalk. The primary entrance is at sidewalk grade and provides direct access to the ground-floor use. The basic architectural elements comprising the storefront are large clear windows, doors with glass, transom windows, and a solid shopfront base. Shopfronts shall also be designed in the following manner:

1. Facades shall be divided into bays, each of which contains its own coherent assembly between piers. Bays are measured between the centerline of each pier.
2. Shopfronts may be combined with frontage elements:
 - a. Dooryard
 - b. Commercial Terrace
 - c. Commercial Court
 - d. Arcade/Gallery

**Table 4.7
Shopfront**

Frontage Elements		Min	Max
A	Height to top of transom	12'	16'
B	Height to bottom of canopy	8'	10'
C	Width of each shopfront bay	10'	25'
D	Height of shopfront base	1'	3'
	Ground floor fenestration (clear)	70%	90%
E	Awning/Canopy Depth	4'	--



Commercial Frontage Variations

The following frontage elements may be combined with Common and Direct Entrances, per the standards of Table 4.4 to form interesting, cohesive, and active ground-floor environments. If utilized, these elements shall comply with the follow standards:

1. The materials, style, and design must be consistent with the architecture of the building.
2. Frontage elements may encroach over the sidewalk in the public right-of-way up to the limit allowed by Table 4.5, subject to the issuance of an encroachment permit or license agreement prior to issuance of building permit.

Commercial Terrace

A commercial terrace is an enclosed area outside of an elevated ground floor. In retail environments, a terrace often provides space for dining and retail display.

Table 4.8
Commercial Terrace Standards

Standards	Min	Max
Depth	8'	--
Finish Level Above Terrace	--	3'
Length of Terrace	--	150'
Distance between Stairways	--	50'
Additional Requirements <ul style="list-style-type: none">• The average grade of the terrace should not differ more than 3 feet from the adjacent right-of-way.• Terraces should feature planters and furniture that provide shade and seating.		

Commercial Court

A court is designed to provide semi-public space, with ground-floor units typically being accessed directly from inside. See further requirements on courts in Section 4.8.4, On-Site Open Space.

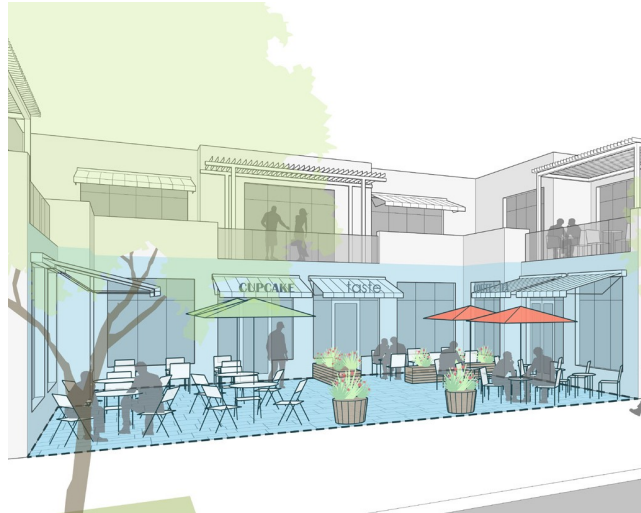


Figure 4.5, Commercial Court

**Table 4.9
Commercial Court Standards**

Standards	Min	Max
Width of Courtyard Area	8'	--
Depth of Courtyard Area	--	3'
Additional Requirements		
<ul style="list-style-type: none"> • Courts should include awnings, canopies, trees, or other shade structures, and seating. • Elevated or sunken courts shall provide direct access from the sidewalk. 		

Gallery

A gallery is a ground-floor colonnade that supports a shed roof or a deck that covers the sidewalk. Galleries provide shade, glare control and weather protection to ground floor shopfronts. Standards for galleries are contained in Table 4.10.

Arcade

An arcade is a façade with a ground floor colonnade that supports upper stories of the building. Arcades provide shade, glare control and weather protection to ground floor shopfronts. Standards for arcades are contained in Table 4.11.

**Table 4.10
Commercial Gallery Standards**

Standards	Min	Max
Height (sidewalk to ceiling)	12'	--
Depth (façade to interior columns)	10'	16'
Setback from edge of curb	3'	6'
Additional Requirements <ul style="list-style-type: none"> • Column height should be between four and five times the column width. • Column spacing should correspond to shopfront openings. • Columns should be appropriately spaced from curbs to allow passengers of cars to disembark. • Walls without openings should not exceed 10 linear feet. 		

**Table 4.11
Commercial Arcade Standards**

Standards	Min	Max
Height (sidewalk to ceiling)	12'	--
Depth (façade to interior column)	10'	16'
Setback from edge of curb	3'	6'
Additional Requirements <ul style="list-style-type: none"> • Column height should be between four and five times the column width. • Column spacing should correspond to shopfront openings. • Columns should be appropriately spaced from curbs to allow passengers of cars to disembark. • Walls without openings should not exceed 10 linear feet. 		

4.7.6 Specific to Residential Frontages

Common Entrance

The Common Entrance provides access to a lobby and/or corridor that services multiple units. Ground-floor dwelling units adjacent to the street should take primary access off the sidewalk via Direct Entrance.

1. Lobbies shall be visible from the right-of-way.
2. Common entrances should be conspicuous and designed as a focal-point of the ground-floor façade.
3. Designs which engage multiple floors are encouraged.
4. Designs which help modulate the façade of the main building are encouraged.
5. Common entrances should provide a comfortable, attractive outdoor public transitional space that allows congregation of the sidewalk. This area of outdoor space should be designed with pedestrian amenities, such as benches or planters.
6. The depth of a recessed entrance may not exceed the width of the opening.
7. Outdoor spaces may be elevated or sunken, accessed by stairs and ramps, and may additionally be enclosed by landscaping.

Table 4.12
Residential Common Entry

Frontage Elements		Min	Max
A	Height to top of transom	10'	20'
B	Height to bottom of awning	8'	--
	Area of outdoor space ¹	80 sf	--
C	Ground floor Fenestration	70%	--
D	Awning/Canopy Depth	4'	--



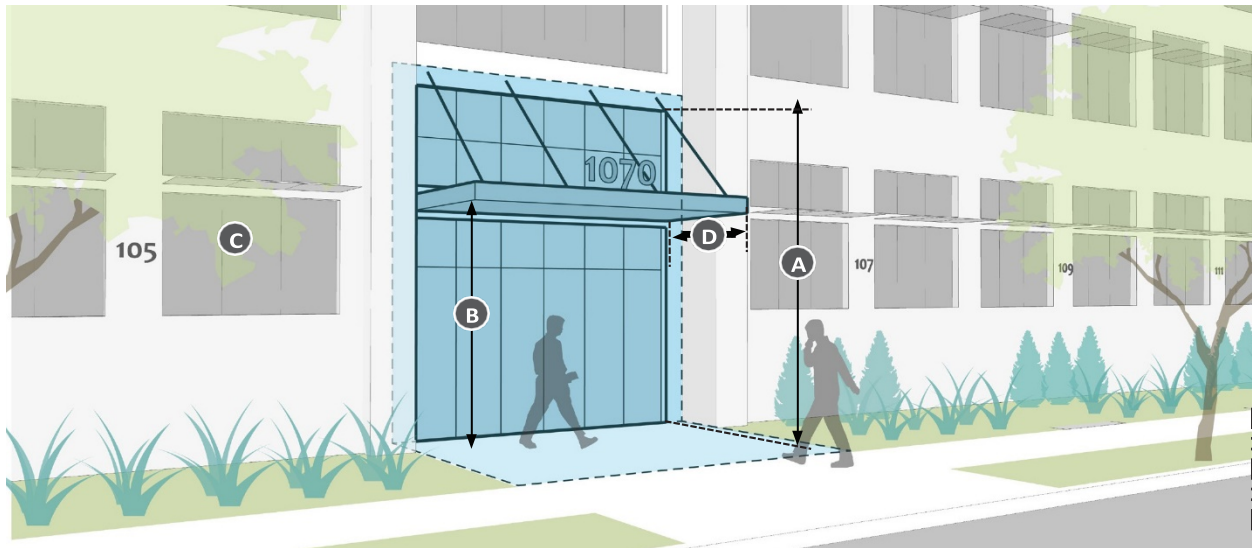


Figure 4.6, Residential Common Entry

Direct Entrance

A residential Direct Entrance is a front door that is raised via a porch, stoop, or terrace. All residential buildings have a primary street setback of at least 10 feet (see Table 4.2).

1. Wall height shall not exceed 3 feet above right-of-way grade.
2. Grading within the private frontage area is limited to between 0.5% and 5%.
3. Frontage elements may encroach into required primary setbacks up to the limit allowed by Table 4.13, subject to the issuance of an encroachment permit or license agreement prior to issuance of a building permit.
4. Landscaping should be provided in the setback areas, and should relate to the street landscaping.
5. Ramps, if provided, should be parallel to the building/façade, with a landscape strip of 1-2 feet between the ramp and the sidewalk.

**Table 4.13
Dooryard**

Frontage Elements		Min	Max
A	Wall height at frontage (total)	--	3'
B	Yard floor height above sidewalk	--	3'
	Yard floor height below sidewalk	--	6'
C	Grading on front yard	0.5%	5%
	Ground floor above grade at setback	18"	3'
Additional Requirements			
<ul style="list-style-type: none"> • Shall be enclosed by low walls. • May be at grade or elevated 			



Figure 4.7, Residential Direct Entry (Dooryard)

Residential Frontage Variations

The following frontage elements may be combined with Common and Direct Entrances, per the standards of Table 4.4 to form interesting, cohesive, and active ground-floor environments. If utilized, these elements shall comply with the following standards:

1. Materials, style, and design should be consistent with the architecture of the building.
2. May encroach over the sidewalk in the public right-of-way up to the limit allowed by Table 4.5, subject to the issuance of an encroachment permit.

Porch

Porches are roofed, unenclosed rooms attached to the exterior of a building that provide access and comfortable semi-private outdoor social spaces that help activate the public realm. Standards for porches are found in Table 4.14.

Table 4.14
Porch Standards

Standards	Min	Max
Porch Depth (excluding stairs) ¹	8'	--
Porch Width	10'	--
Porch Height ²	8'	12'
Floor Height ³	1.5'	3'
Between porch and front property line	5'	--
Additional Requirements		
<ul style="list-style-type: none">• Porches may be used to provide access to multiple front doors of ground floor units.• Porches may also wrap around building corners. This is particularly appropriate on corner lots.		
Notes:		
<ol style="list-style-type: none">1. Between building façade and end of porch deck.2. From porch floor to top of porch columns3. Measured from grade at right-of-way		

Stoop

A stoop is a stairway and landing leading directly from the right-of-way to an elevated building entrance. Standards for stoops are found in Table 4.15.

Residential Terrace

A residential terrace is an enclosed area outside of an elevated ground floor, usually enclosed by a low wall or fence. Standards for residential terraces are found in Table 4.16.

**Table 4.15
Stoop Standards**

Standards	Min	Max
Stoop Width	4'	8'
Stoop Depth (not including stairs)	4'	8'
Stoop Floor Height ¹	1.5'	3'
Planter/Fence Height	--	3'
Entry Recession Depth	0.5'	6'
Additional Requirements		
<ul style="list-style-type: none"> • The exterior stairway may be perpendicular or parallel to the adjacent sidewalk. When parallel, landscaping of 1 to 2 feet should be provided between the side of stair/stoop and the sidewalk. • Adjoining stoops should be limited to two entries. A stoop may also provide access to a common entry. • Gates are discouraged. 		
Notes:		
1. Measured from grade at right-of-way.		

**Table 4.16
Residential Terrace Standards**

Standards	Min	Max
Depth, Clear	8'	--
Finish Level Above Sidewalk	--	3'
Length of Terrace	--	150'
Distance between Stairways	--	50'
Additional Requirements		
<ul style="list-style-type: none"> • Residential terraces which do not provide direct access to the public sidewalk may be approved, subject to Director's Reviews. • Door and window design should be appropriate for street-fronting facades. 		

Residential Court

A residential court is a landscaped, semi-public common space, with ground floor units typically accessed directly from within. This frontage must be applied in combination with one or more additional frontage elements. Standards for residential courts are found in Table 4.17.

Table 4.17
Residential Court Standards

Standards	Min	Max
Width of courtyard area (clear)	30'	50'
Depth of courtyard area (clear)	15'	60'
Additional Requirements <ul style="list-style-type: none">• Outdoor spaces associated with courts may be elevated or sunken, accessed by stairs, and may additionally be enclosed by a low landscape wall or hedge.• Courts should include awnings, canopies, trees, and other shade structures.		

4.8 Access & On-Site Open Space

4.8.1 Applicability

Each addition, new building, or building that renovates over 50 percent of the floor area is required to comply with the standards of this section. At-grade open space must be directly accessible from the adjacent ground floor. The dimensions in Table 4.18 reflect the required dimensions to count towards minimum area; see standards for each on-site open space type.

4.8.2 On-Site Open Space Design

On-site open space contributes to the character of an individual building and often to that of the street. Several types of spaces can be used within and around buildings to provide light, air, and visual respite.

The need for on-site open space is greater for residential units than for other nonresidential uses. Thus, any building that contains residential units, including mixed-use housing in the District Core or District General, is required to provide open space in the forms prescribed in this section, while buildings that do not contain any residential units are not required to provide on-site open spaces.

4.8.3 Standards and Guidelines for All

The following pages provide standards and guidelines for on-site open space requirements and types.

1. Residential buildings must provide on-site open space.
2. Residential buildings in the District Edge shall provide on-site open space in the form of one or more of the defined strategies: rear yard, side yard, or court.
 - a. Residential buildings in the District Edge may additionally have decks and balconies, but these types do not meet the on-site open space requirement for residences in the District.
3. Residential mixed-use buildings in the District Core and District General shall provide on-site open space in the form of one or more of the defined strategies: deck, balcony, or court.
 - a. Residential mixed-use buildings in the District Core shall not have side or rear yards.
4. In the event that a non-residential building intends to provide on-site open space, the recommended forms are deck, balcony or court.

5. Frontage elements, such as dooryards, porches, and stoops, as well as architectural elements such as canopies and balconies, may encroach into on-site open space.

4.8.4 On-Site Open Spaces

Table 4.18
On-Site Open Space Types

On-Site Open Space Types	District Zones			Minimum Dimensions	Reference
	DC	DG ¹	DE		
Rear Yard		o	•	15' x 15'	
Side Yard		o	•	12' x 20'	
Court	•	•	•	15' x 25'	
Deck	•	•	o	15' x 15'	
Balcony	•	•	o	6' x 8'	

Notes: o = Allowed; • = Recommended

1. Allowed types may only be used in combination with one or more of the recommended types to fulfill requirements.

Rear Yard

A rear yard is a private, landscaped space located behind a single family or multi-family building that is for the use of the residents of one or more dwellings. For buildings with two or more units, rear yards may be divided into separated private yards, provided each private yard is located directly adjacent to the dwelling unit. Rear yard standards are provided in Table 4.19.

Table 4.19
Rear Yard Standards

Standards	
Size	15' x 15'
Additional Requirements	
<ul style="list-style-type: none"> • Configuration. Rear yards shall be located behind the primary building, away from the view of the Primary Street. For buildings with two or more units, rear yards may be divided into separated private yards, provided the private yards are directly adjacent to the unit. 	

Side Yard

A side yard is a landscaped space along one side of a lot. Side yards may be semi-private spaces through which visitor access is provided to one or more buildings or dwellings, or may be private spaces for exclusive use of residents. Side yard standards are provided in Table 4.20.

Table 4.20
Side Yard Standards

Standards	Min	Max
Width	12'	--
Depth	20'	--
Setback	--	15'

Additional Requirements

- Access. The side yard must either border the public sidewalk of the Primary Street or be accessed from the public sidewalk via a continuous ADA compliant pedestrian path, uninterrupted by vehicular travel paths.
- Frontage. Buildings fronting Side Yards shall also provide an entry on the active frontage façade facing the Primary Street.
- Enclosure. One side of the perimeter shall be defined by building face. The opposite side may be defined by a building wall or landscape element on an adjoining property.

Court

A court is a semi-public, shared open space within a lot, for use by more than one tenant. It is a well-defined, coherent area that is an essential component of the project's design, not merely space left over after the building mass is placed. Courts generally provide visitor access from the street to dwellings, retail or office spaces, or buildings within the lot that lack direct frontal access from the street. Court standards are provided in Table 4.21.

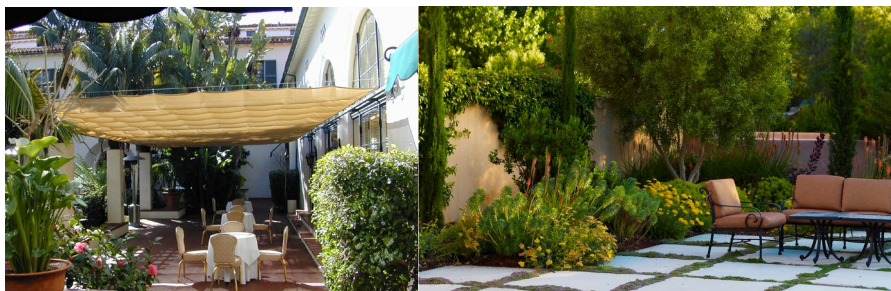


Table 4.21
Court Standards

Standards	Min	Max
Width	12'	--
Depth	20'	--
Setback	--	25'
<p>Additional Requirements</p> <ul style="list-style-type: none"> • Access. The court must either border the public sidewalk of the Primary Street or be accessed from the public sidewalk via a continuous ADA compliant pedestrian path, uninterrupted by vehicular travel paths. • Frontage. Units may front the court, as long as the building provides a clearly demarcated entry on the façade facing the Primary Street. • Enclosure. A court's perimeter shall be defined by building facades on 3 sides, except on corner lots, where 2 court sides may be building faces and 2 sides may be streets. In all cases, one side may be defined by a building wall or landscape element on an adjoining property. The remaining side meets the street, with, or without a street setback. • Private Area. Courts should be designed to provide for private access to dwellings and businesses that lack direct street frontage. Courts should also provide space for private outdoor space in the form of private patio and terrace spaces. • Amenities. Courts should provide a central, flat area with both paving and landscaping that encourages human activity and interaction. Courts should include public amenities such as seating, fountains, BBQ islands, or outdoor fireplaces. • Landscape. Courts should have trees, plants, paving, decomposed granite or other appropriate ground. Trellises and raised planter walls are encouraged. See Appendix A. 		

Deck

A deck is an accessible open space on a portion of the building floor plate above ground floor. Individual units may have decks, or they may be available to all building tenants/residents. When on the top of a building, these are called roof decks.

Table 4.22
Deck Standards

Standards	
Size	15' x 15'
Additional Requirements	
<ul style="list-style-type: none">• Configuration. Decks may be located on a portion of a building, or if on the roof, they may cover all of a building's footprint subject to the California Building Code. Decks are outdoor.• Amenities. Decks should provide seating areas, shading elements, and landscaping, often in the form of planter boxes. Amenities may include design elements such as fountains, outdoor fireplaces, or community gardens to encourage their use as outdoor rooms or gathering places.• Finishes. Materials, finishes, fixtures, and colors visible from the street should be designed in a manner that is consistent with the architectural language of the building.	

Balcony

A balcony is a small open space that may be assigned to an individual unit or a shared open space available for use by more than one unit.

Table 4.23
Balcony Standards

Standards	
Size	6' x 8'
Additional Requirements	
<ul style="list-style-type: none">• Configuration. Balconies may project from the façade or be recessed.• Finishes. Materials, finishes, fixtures, and colors visible from the street should be designed in a manner that is consistent with the architectural style of the building.	

4.9 Signs

4.9.1 Overview

This section establishes the standards for on-site signs. The term “recommended” denotes a guideline and is only advisory.

4.9.2 Applicability

Each new sign, new building, or façade renovation is required to be designed in compliance with the standards and guidelines of this section. Each building may apply the allowed signage in compliance with the standards per type, as allowed in Table 4.24. Only signs allowed in this section may be permitted in the Lancaster Health District. Allowed sign types may be combined unless stated otherwise.

4.9.3 Design Guidelines for All Signs

The following design guidelines may be used in reviewing the design of individual signs. Substantial conformance with each of the following design criteria is recommended before issuance of a building permit.

1. In general, the sign should:
 - Be on the building entrance façade
 - Not obscure architectural elements or passageways
 - Take up less than 10% of the façade surface area
 - Storefront signs should be placed between the first and second floors.
2. Colors in the sign should:
 - Be harmonious; not have distracting colors
3. Construction of the sign should:
 - Be of permanent, high-quality material
 - Be permanently attached to the building
 - Be designed by a professional such as an architect, building designer, interior designer, etc.
4. Materials and Structure of the sign should:
 - Match the building style
 - Not have reflective material (except minimally in window signs)
 - Be durable to weathering
 - Have proportional supporting members
 - Have individual letters incorporated into the façade

5. Sign Copy (Messaging) should:
 - Relate only to the name or nature of the business
 - Be brief, concise, and legible
 - Occupy less than 40% of the background area
6. Sign Lighting should:
 - Minimize glare
 - Not blink, flash, flutter or change light intensity or color
 - Not create a hazard for pedestrians or drivers
 - Have hardwired fluorescent light sources or similar efficient lighting
7. Directory Signs should:
 - Be located in the lobby or central court of a building complex
 - Be externally illuminated
8. Sign Maintenance should:
 - Be in good repair and functioning at all times
 - Have repairs that are of equal or better quality than the original
 - Have all appendages removed when sign is removed

Table 4.24
Allowed Sign Types

Type	District Zones			Set Reference for Guidelines
	DC	DG	DE	
Wall	P	P	N/A	Figure 4.9.4A
Window	P	P	P	Figure 4.9.4B
Front Yard	N/A	P	P	Figure 4.9.4C
Projecting	P	P	P	Figure 4.9.4D
Sidewalk	P	N/A	N/A	Subject to Director's Review
Roof	P ²	N/A	N/A	Subject to Director's Review
Mural	P	N/A	N/A	Subject to Director's Review
Theater Marquee	P	P	N/A	Subject to Director's Review
Notes:				
P = allowed subject to applicable requirements				
N/A = not allowed				
1. Subject to the City of Lancaster's processing requirements.				
2. Only for buildings at least 5 stories tall				

Wall Sign

A wall sign is a painted wall or reverse-channel sign applied directly to the façade, typically above the storefront or more creatively as approved by the City. This type consists of either a single externally illuminated panel or individual letters and/or logo. This type of sign is intended for viewing from across the street

Table 4.25
Wall Sign Standards

Standards	Min	Max
Height	10"	24"
Width (as % of façade width)	--	60%
Clearance from openings	24"	--
Additional Requirements <ul style="list-style-type: none">• Up to one wall sign may be approved per business per street-facing façade. In multi-tenant buildings, only the businesses with frontage on the sidewalk may have a wall sign.• Wall signs shall be located above the storefront and at least 12 inches from any eave, edge of building or top of parapet. On multi-story buildings, wall signs should be located either above the storefront or above the openings on the uppermost story.• Sign thickness (as measured from the wall) shall not exceed four inches.• Internal illumination is prohibited.• Lettering on background panel may be up to 18 inches tall. If a background panel is not included, letter height may be up to 24 inches tall.		

Window Sign

A window sign is a sign painted or applied directly to the storefront windows and/or doors. This type typically consists of individual letters and a logo with allowances for some background. Window signs also include posters for advertisements and sales, product merchandise posters, open/closed signs, and painted or etched business names and logos.

Table 4.26
Window Sign Standards

Standards	Min	Max
Height (% of window/door height)	--	50%
Width (% of window/door width)	--	50%
Area (% of window/door area)	--	50%

Additional Requirements

- One window sign is allowed per storefront.
- Window signs may not occupy more than 25% of a window.
- Permanent window signs shall be individually painted, etched, or otherwise applied letters or logo graphics surrounded by clear glass.
- Exposed neon signs are subject to Director approval.



Window Sign



Reflective material is only appropriate in window signs and should be minimally used.

Front Yard Sign

A front yard sign is intended for businesses in buildings that are set back from the public right of way. The target viewer is the passing pedestrian or bicyclist. They may be freestanding or integrated into the design of a fence or dooryard wall.

Table 4.27
Front Yard Sign Standards

Standards	Min	Max
Sign Width	--	30"
Sign Height	--	30"
Overall Height	--	5'
Vertical Clearance from Sidewalk	2'	--

Additional Requirements

- Signs shall not encroach into the public right-of-way.
- Signs must be oriented toward side property lines.
- Signs shall be of painted wood or other high quality and durable material.
- Exterior illumination is permitted, provided that it is from a single bulb, shielded, and warm in tone.



Front yard sign for a house converted to an inn.

Projecting Sign

A projecting sign is a double-sided sign oriented perpendicularly from the building façade and attached either by a mounted wall brace or from the ceiling of a balcony or arcade. Projecting signs typically project over a public right-of-way such as a sidewalk or public open space and are intended for viewing by approaching pedestrians.

Table 4.28
Projecting Sign Standards

Standards	Min	Max
Sign Width	--	30"
Sign Height	--	30"
Overall Height	--	5'
Vertical Clearance from Sidewalk	2'	--
Additional Requirements <ul style="list-style-type: none">• One projecting sign is allowed for every storefront entrance adjacent to the sidewalk.• Signs should be mounted near storefront entrances.• An encroachment permit is required prior to installation of any sign extended into the public right-of-way.• Projecting signs that hang from the ceiling of a balcony or arcade should be centered within the balcony or arcade.• Projecting signs shall be externally illuminated by a light mounted on the façade or by neon tubing used to illuminate letters, symbols, and accent frames.• Projecting signs should not be placed under an awning or horizontally within five feet of an awning or another projecting sign.		

4.10 Street Design Standards

4.10.1 Introduction

This section establishes the standards for thoroughfares with the goal of generating a high level of connectivity within the Health District through the creation of a public realm network that functions well for pedestrians, bicyclists, drivers, transit users, and those operating emergency vehicles. The most current edition of the National Association of City Transportation Officials' (NACTO) Urban Street Design Guide, Lancaster Municipal Code and the Engineering Design Guidelines shall be consulted for all fine-tuning and review. Where conflicts existing, the City's Municipal and Engineering Design Guidelines take precedence.

4.10.2 Applicability

The design of each new street, and the improvement of existing streets shall be designed (or redesigned) in compliance with the standards and guidelines of this section. The City's Traffic Engineer has final approval of all street improvements for new or existing streets and may approve deviations from the standards identified in this section.

4.10.3 Intersections

- A. Turning Radii. The effective turning radius takes parked cars and bike lanes into account. Within the Plan Area, new turning radii and improved turning radii are limited to 25 feet (actual) and 32 feet (effective) turning radius. Where actual and effective radii are in conflict with the targeted improved radii value, the most restrictive of the two standards shall prevail.
- B. Pedestrian Crossings. Crosswalks shall be in accordance with the City of Lancaster Municipal Code and Design Guidelines.
- C. Roundabouts. Roundabouts shall be designed in accordance with the National Cooperative Highway Research Program's Report 672, "Roundabouts: An Informational Guide," 2nd Edition, by an engineer with extensive experience in modern roundabout design. It shall also be demonstrated that it has been designed to prioritize non-motorist comfort and safety, incorporating pedestrian crosswalks and slowing vehicular traffic for the duration of its path through the roundabout.

4.10.4 Public Frontage

- A. Bicycle lanes. Where appropriate, physical buffers should be provided. In addition to the standards provided herein, all proposed bike lane designs shall be reviewed against the standards established by Caltrans and the City of Lancaster.
- B. Landscaping. Specific standards regarding allowed tree types and locations, and all other landscaping in the public realm are found in Appendix A: Landscape Guidelines. All landscaping must meet the City of Lancaster line of standards for medians and corners. Additionally, any deviations from the plants identified in Appendix A are at the discretion of the Development Services Director.
- C. On-Street Parking. On-street parking, where provided, shall be placed so as not to infringe upon safe pedestrian visibility at corners.
- D. Parking Lane Planters. As shown in the Thoroughfare Standards (4.10.5), planters are required in some parking lanes. Where required, they shall extend 7 feet from the curb face and occur at a rate of one per 100 feet of block face.
- E. Furniture. Sidewalk benches, where provided, should either be located on the building side of the sidewalk facing out, or on the street side of the sidewalk perpendicular to the street.
- F. Lighting. Lights shall be located at the midpoints between street trees and shall comply with the City of Lancaster street light standards.
- G. Street Signs. Clearly visible pole-mounted street signs shall be provided at all intersections in accordance with City policy. Signs shall be of a unified design throughout the Lancaster Health District.
- H. Curb Paint. Where deemed necessary by the City Engineer or Fire Marshal to restrict curbside parking, the curb paint shall be done in accordance with City of Lancaster and Los Angeles County Fire Department standards.
- I. Ramp Alignment. Sidewalk ramps shall be aligned with the direction of the sidewalks.



Bike lane improvements.



Desert landscaping in Landscape/Parkway zone, with a pedestrian path connecting sidewalk and street.



Generous outdoor street furniture.

4.10.5 Thoroughfare Standards

Through its configuration and design, the network of public and private streets is intended to generate the pedestrian-oriented public realm framework of the district, supporting and enhancing the Lancaster Health District character, as intended by the vision described in Chapter 3. All public streets shall be designed to the standards in the Lancaster Municipal Code. Private streets may incorporate the design aspects identified in this Chapter with the approval of the City Engineer. The design of each new street and the individual design components thereof shall be based on applicable street types, open space standards, and other applicable sections.

Each Street Classification has applicable types of thoroughfares as shown in Table 4.29. These street types will be matched with existing City of Lancaster street classifications. Arterial retrofits refer to existing arterials, and the standards regulate changes and improvements to these existing streets. Avenues and Health District Streets refer to new and existing streets that traverse the Plan Area. Frontage Road refers to a specific condition on the parcel containing the current hospital, south of Avenue J and west of 15th Street West. Access Streets and Paseos are the remaining types that are appropriate for Tertiary Streets. Fire Lanes/Alleys will service the backs of buildings, and exist as a thoroughfare type, but do not fall under any of the street classifications because they do not fall under any of the street classifications because they do not comply with the same standards that correspond with Streets, such as Active Frontage (Section 4.7) and block perimeters (Section 4.3).

In the thoroughfare illustrations in this section, lane lines and centerlines are shown for clarity of functional areas and dimensioning, but are not intended to illustrate the final striping plans. The following apply to all thoroughfares:

- A. Design Speed. Target vehicular speeds should be met not only by regulatory signs, but through the strategic design of elements that control and modify traffic flow.
- B. Centerlines. Centerlines will be installed to match City of Lancaster requirements.

**Table 4.29
Thoroughfare Types**

Type	Active Street	Block Side	Street Classification			See Reference for Standards
			Primary	Secondary	Tertiary	
Arterial Retrofit	P	P	Existing Streets, see Section 4.10.6			
Avenue	P	P	O			See Section 4.10.7
Health District Street	P	P	O	O	O	See Section 4.10.8
Frontage Road	P				O	See Section 4.10.9
Access Street		P			O	See Section 4.10.10
Paseo	P	P			O	See Section 4.10.11
Fire Lane/ Alley			See Section 4.10.12			
Notes: O = Allowed; P = Permitted 1. Requires active frontage, so buildings must be oriented or designed to provide access to these thoroughfares. 2. Can act as the side of a block, effectively breaking up large parcels.						

4.10.6 Arterial Retrofit

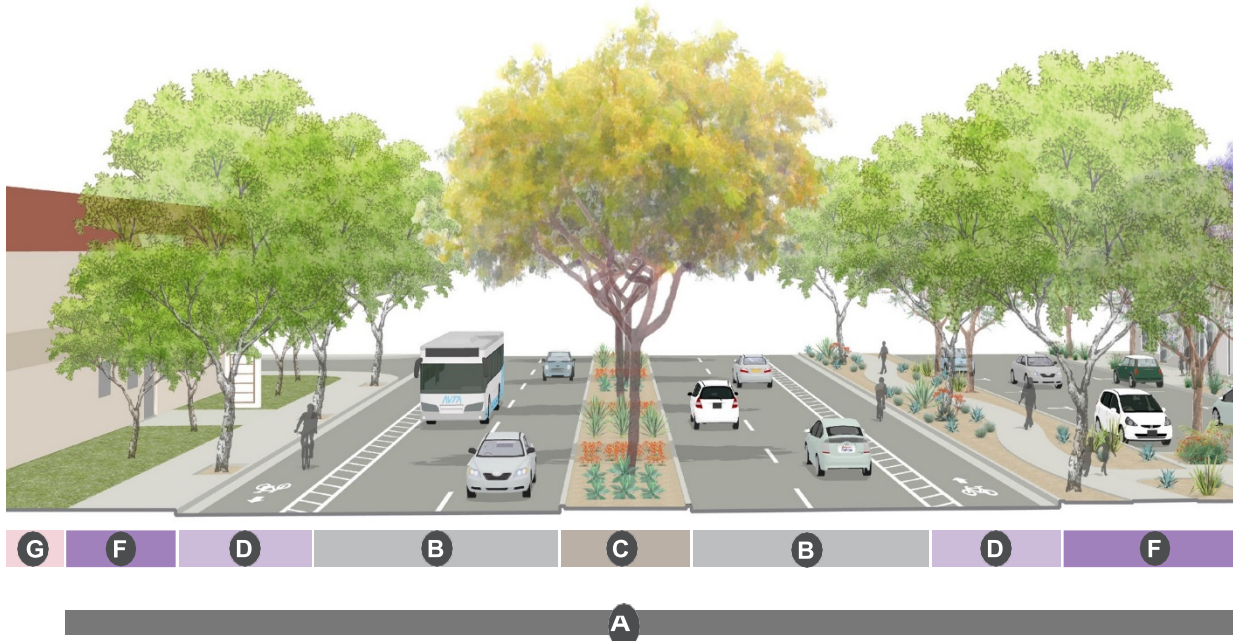


Figure 4.8, Avenue J Retrofit

Intent – Avenue J

Retrofits to Avenue J will ultimately be coordinated with the Avenue J Interchange improvements. Additional streetscape and landscape improvements may be included if they fit within the plan area.

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	Per Existing
	Curb-to-Curb	Per Existing
B	Travel Lanes	4 (2 each way); 11' lane width
C	Median	4'-12'; raised
D	Bicycle Facilities	Class II Bike Lanes
Public Frontage		
E	Flex Curbside Zone	No parking
F	Landscape/ Furniture zone	Varies; street trees and native landscaping; See Appendix A
	Sidewalk	Meandering sidewalk per City policy
	Drainage Type	Per existing City of Lancaster requirements
Private Frontage		
G	Frontage Type	See Section 4.7

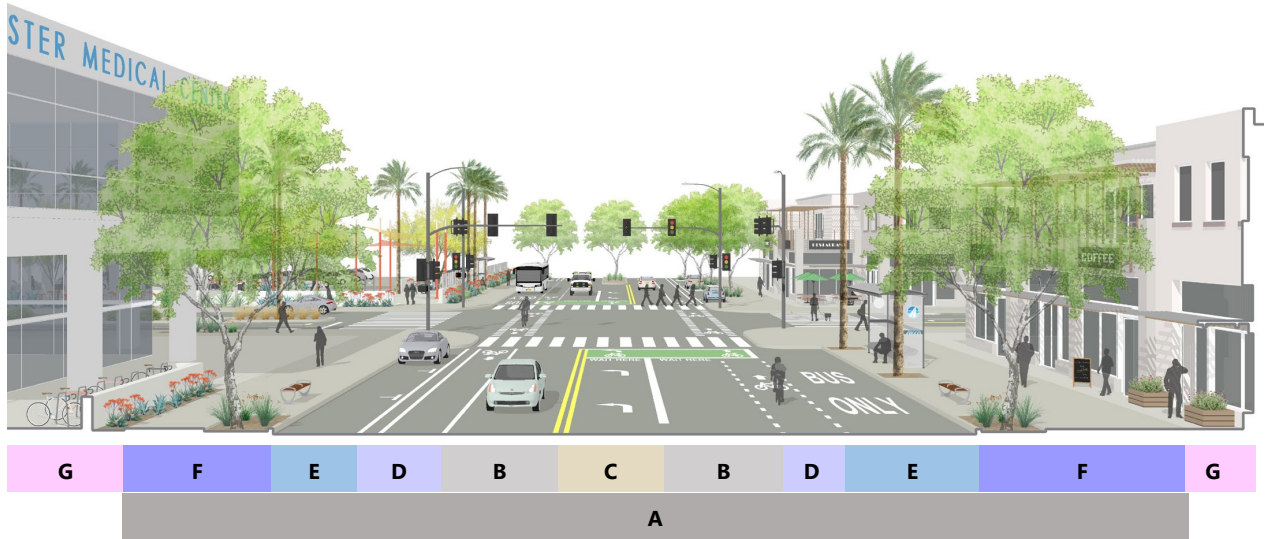


Figure 4.9, 15th Street West

Intent – 15th Street West

15th Street West is an important north-south street. Where possible the construction of a landscaped median, addition of parallel on-street parking, and bike lanes would occur in support of street fronting businesses and/or residences, such that 15th Street West acts as a “zipper” reconnecting the east neighborhoods to the central district, rather than the barrier in its current form.

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	Per Existing
	Curb-to-Curb	Per Existing
B	Travel Lanes	4 (2 each way); 11’ lane width
C	Median	10’ painted median
D	Bicycle Facilities	Class II Bike Lanes
Public Frontage		
E	Flex Curbside Zone	Parallel parking; 8’ wide
F	Landscape/ Furniture zone	Varies; street trees and native landscaping; See Appendix A
	Sidewalk	5’ wide minimum, meandering
	Drainage Type	Per existing City of Lancaster requirements
Private Frontage		
G	Frontage Type	See Section 4.7

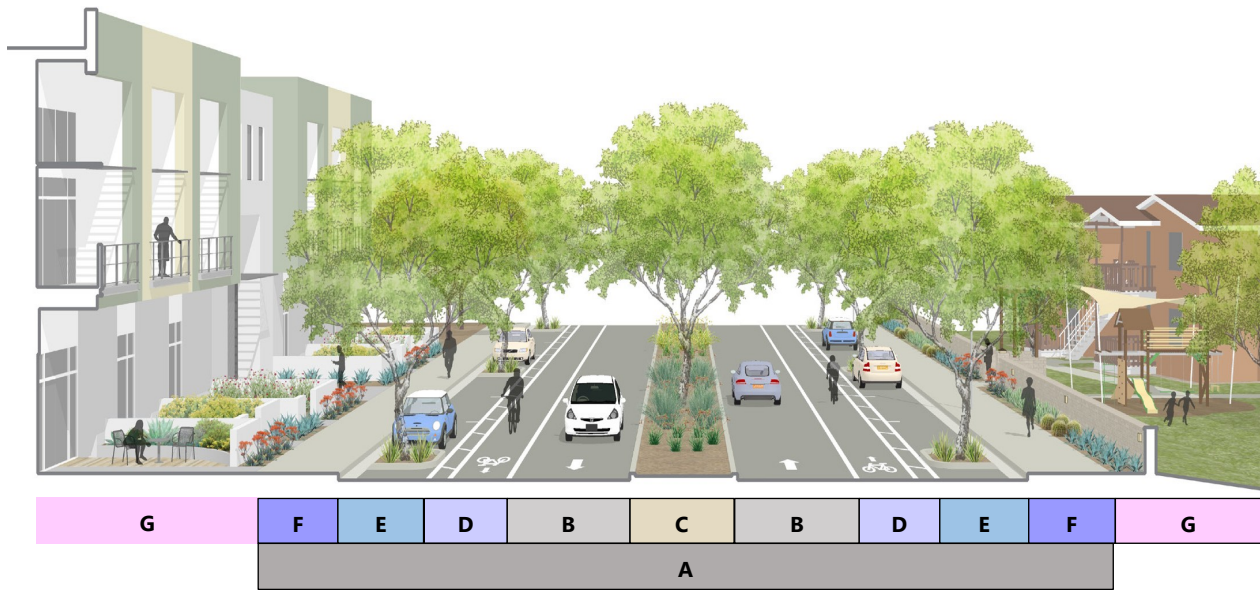


Figure 4.10 Avenue J-8 Retrofit

Intent – Avenue J-8

Avenue J-8 is effectively a neighborhood street that widens to the City’s arterial standards west of 15th Street West. This plan recommends that it continue as a 3-lane neighborhood street until 20th Street West to better connect the Central District to the properties and businesses south of Avenue J-8. Proposed retrofits include maintaining one travel lane in each direction across the full length of Avenue J-8 in the Plan Area (instead of just east of 15th Street West), the formalization of a Class II bike lane and introduction of parallel on-street parking.

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	Per Existing
	Curb-to-Curb	Per Existing
B	Travel Lanes	2 (1 each way); 11’ lane width
C	Median	10’ painted median
D	Bicycle Facilities	Class II Bike Lanes
Public Frontage		
E	Flex Curbside Zone	Parallel parking; 8’ wide
F	Landscape/ Furniture zone	None
	Sidewalk	5’ wide minimum
	Drainage Type	Pervious paving or reverse crown (drains to adjacent street gutters)
Private Frontage		
G	Frontage Type	See Section 4.7

4.10.7 Avenues

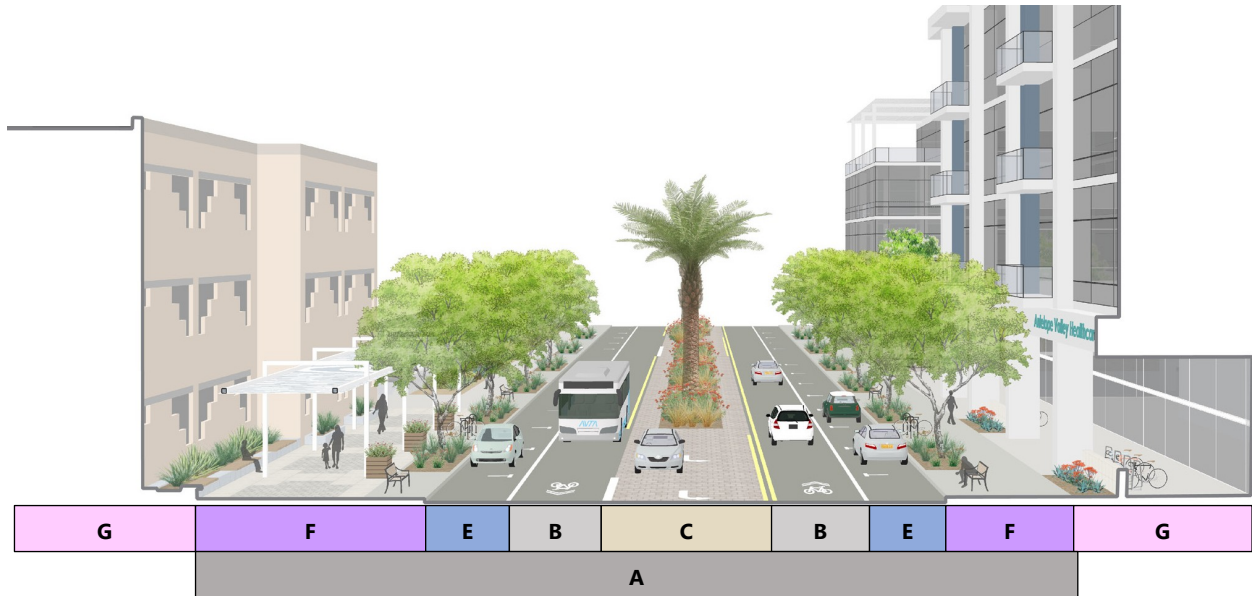


Figure 4.11, Avenue J-5

Intent

The new Primary Streets that will connect through the Health District will take the form of Avenues and Streets. Avenues are wider thoroughfares that connect from one perimeter arterial of the plan to the opposite perimeter. Organized, aesthetically appealing entry avenues will bring traffic into the Lancaster Health District. Avenues are distinguished from other streets in the Plan by having 2 lanes of traffic with a landscaped median and protected turn lane, Class III Bike Lanes, and parallel on-street parking. The wider street section makes larger buildings that front the streets appropriate.

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	Per Existing
	Curb-to-Curb	Per Existing
B	Travel Lanes	2 (1 each way); 11' lane width
C	Median	10' minimum; no raised curb; street trees and native landscaping; See Appendix A
D	Bicycle Facilities	Class III Bike Lanes (Sharrow)
Public Frontage		
E	Flex Curbside Zone	Parallel parking; 8' wide
F	Landscape/ Furniture zone	Varies; street streets and native landscaping; See Appendix A
	Sidewalk	5' wide minimum
	Drainage Type	Per existing City of Lancaster requirements
Private Frontage		
G	Frontage Type	See Section 4.7

4.10.8 Health District Street A

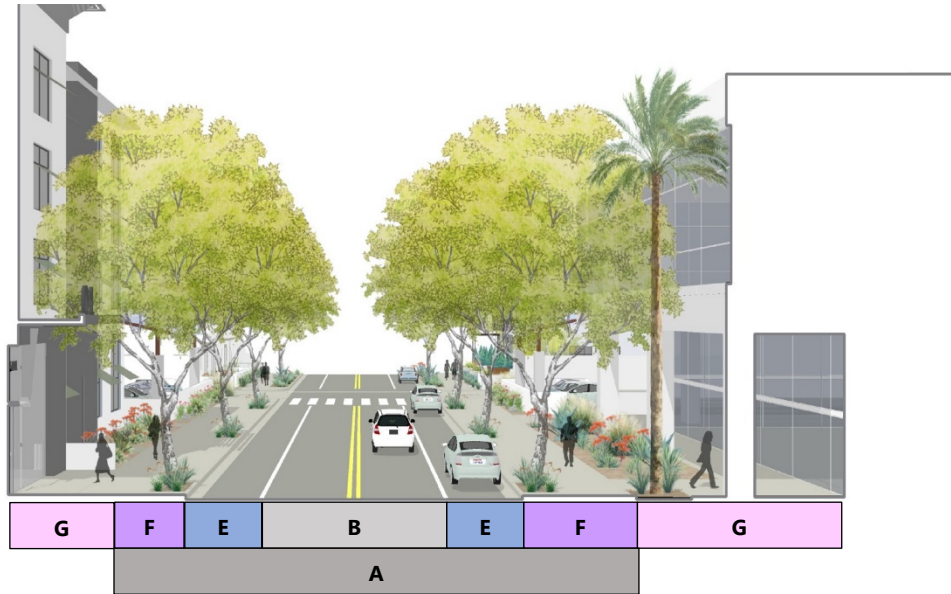


Figure 4.12, Typical Commercial Street (DC/DG Zone)

Intent

Health District Streets are a type of thoroughfare that can be designed for Primary, Secondary, or Tertiary streets. Health District Streets take the form of 2 drive lanes, with a possible median, Class III bike lanes, and parallel on-street parking. This type can be used to create passage through the length, connect from arterials to avenues, or as small connections between avenues entirely within the plan area. These streets serve well both commercial and residential frontages and ground floor uses. These design standards apply regardless of whether the ground floor use is residential or commercial (Figures 4.12 through 4.14).

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	60' minimum
	Curb-to-Curb	36' minimum
B	Travel Lanes	2 (1 each way); 10' lane width
C	Median	Where possible
D	Bicycle Facilities	Class III Bike Lanes (Sharrow)
Public Frontage		
E	Flex Curbside Zone	Parallel parking; striped at 8' wide
F	Landscape/ Furniture zone	Varies; street trees and native landscaping; See Appendix A
	Sidewalk	5' wide minimum
	Drainage Type	Per existing City of Lancaster requirements
Private Frontage		
G	Frontage Type	See Section 4.7

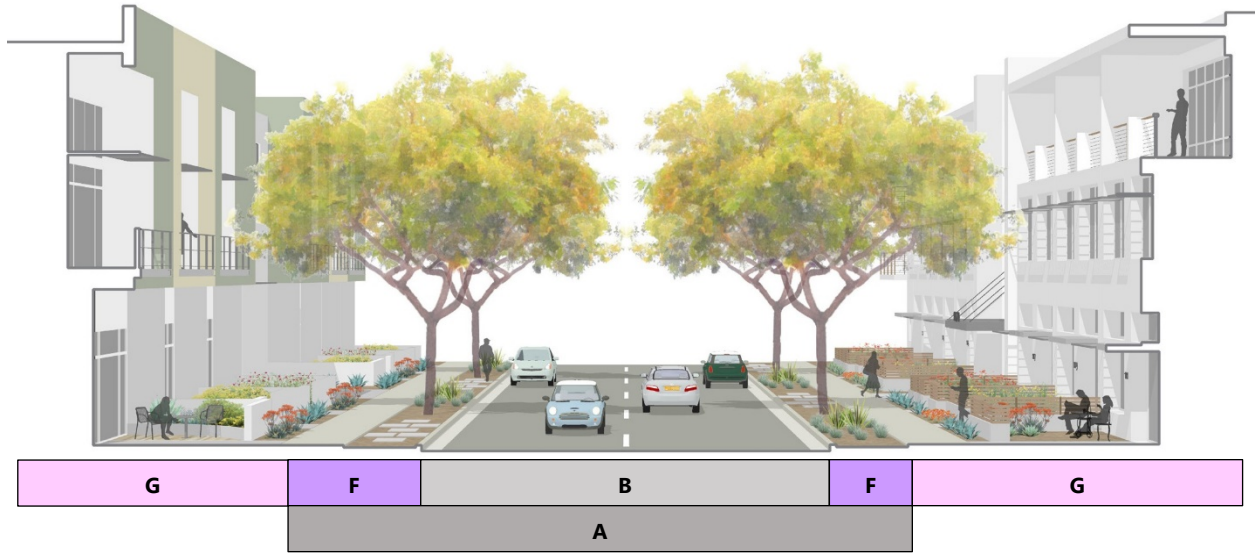


Figure 4.13, Typical Residential Street (DG Zone)

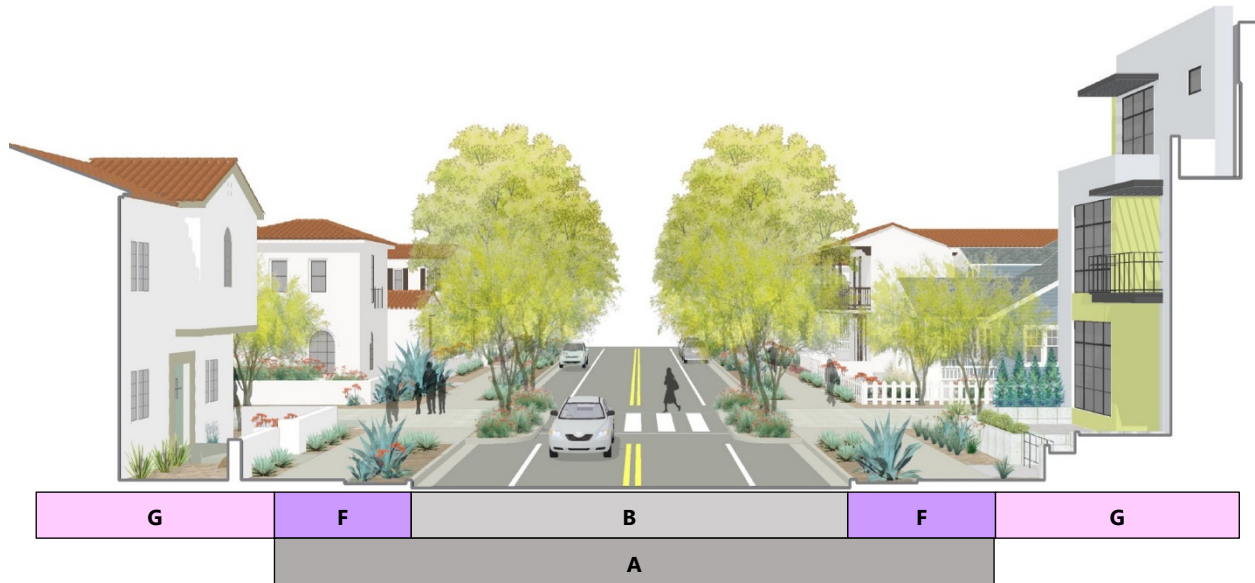


Figure 4.14, Typical Residential Street (DE Zone)

4.10.9 Frontage Road

Intent

The frontage road that is proposed adjacent to Avenue J is a strategy for providing better frontage for the building developments that will occur on Avenue J. Because Avenue J has high-speed traffic and there is no on-street parking, it is not conducive to retail success. A one-way slip lane, defined by its adjacency to a major thoroughfare, provides the opportunity to create lower-speed traffic, on-street parking, and because of these changes, a safer and welcoming pedestrian environment. Wide sidewalks and landscaping to provide shade are encouraged.

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	60'-68' (includes buffer sidewalk)
	Curb-to-Curb	36'
B	Travel Lanes	2 (1 each way); 10' lane width
C	Median	None
D	Bicycle Facilities	Class III Bike Lanes (Sharrow)
Public Frontage		
E	Flex Curbside Zone	Parallel parking; striped at 8' wide
F	Landscape/ Furniture zone	Varies; street streets and native landscaping; See Appendix A
	Sidewalk	5' wide minimum
	Drainage Type	Per existing City of Lancaster requirements
Private Frontage		
G	Frontage Type	See Section 4.7

4.10.10 Access Street

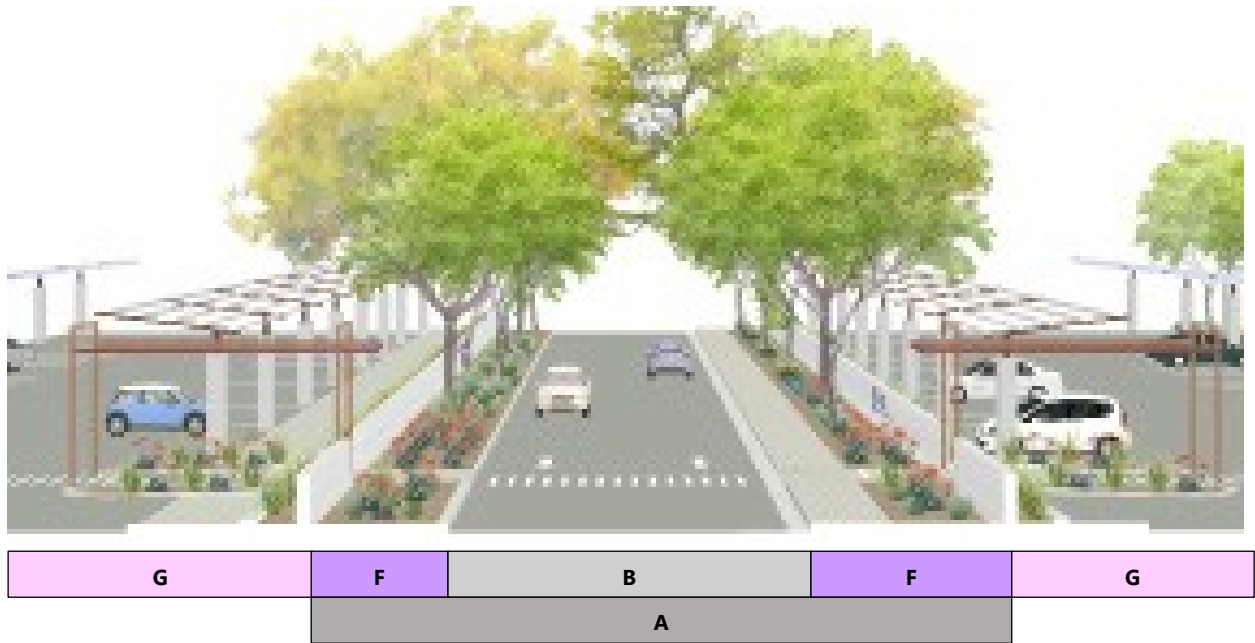


Figure 4.15, Access Street

Intent

The access street is specifically intended for the blocks in the northeast corner of the District Core subzone (site of the existing hospital) where a significant amount of surface parking is anticipated in the interior of the block. This street provides access to these parking areas while also providing the pedestrian facilities of a traditional street in a safe, comfortable, attractive manner.

While this street type does not anticipate that buildings will line it in the near-term, it should be designed to the standards of all other streets in the Plan Area such that if/when future development becomes a higher priority than surface parking, the street/block can be easily retrofitted.

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	50'-60'
	Curb-to-Curb	26'-28'
B	Travel Lanes	2 (1 each way); 13'-14' lane width
C	Median	None
D	Bicycle Facilities	Class III Bike Lanes (Sharrow)
Public Frontage		
E	Flex Curbside Zone	No parking
F	Landscape/ Furniture zone	8'-10'; street tree and native landscaping; See Appendix A
	Sidewalk	5' wide minimum; only required on one side
	Drainage Type	Per existing City of Lancaster requirements
Private Frontage		
G	Frontage Type	N/A

4.10.11 Paseo

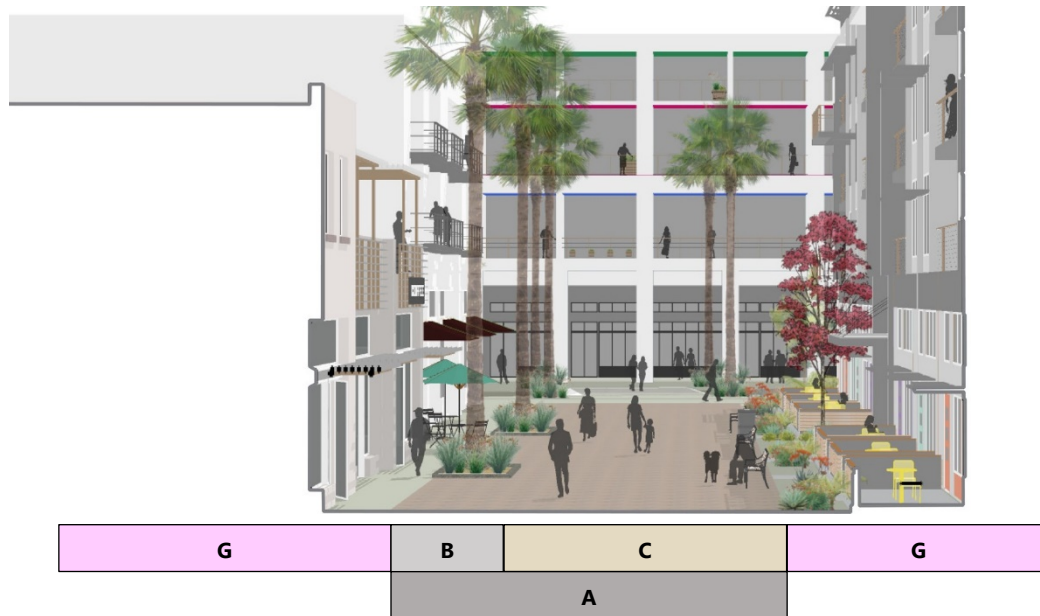


Figure 4.16, Commercial Paseo

Intent

A Paseo is a strictly pedestrian thoroughfare that cuts through a block. A Paseo can take many different forms, dependent on the form and use of the block that it bisects. Buildings should provide active frontage on the paseo. A Paseo counts as a Tertiary Street in that it can define a block size and may be used to break up the sizes of parcels into conforming block sizes.

Commercial paseos exist between shops or restaurants and often provide a convenient route from the large rear parking lots to the shopping street. Paseos also provide additional merchandising opportunities, as shops and restaurants may open directly to them with display windows or in some cases outdoor dining space in a shady and wind-protected public open space.

Commercial paseos may be paved or a combination of paved and landscaped. In all cases, paseos should be shaded. If building heights relative to paseo width do not provide sufficient shading, trees and shade structures should be incorporated into the Paseo design.



Figure 4.17, Residential Paseo

Residential paseos provide pedestrian shortcuts through longer blocks, while also providing small neighborhood gather or play spaces. Adjacent homes face and overlook the street and the paseo, similar to homes on street corner lots, providing the paseos with a safe, comfortable character with residents’ “eyes on the street.” Landscaping may flank a single central walk, or a small open space may be located between a pair of walks.

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	10'-30'
	Curb-to-Curb	N/A
B	Travel Lanes	N/A
C	Median	N/A
D	Bicycle Facilities	N/A
Public Frontage		
E	Flex Curbside Zone	N/A
F	Landscape/ Furniture zone	Not required; street trees and native landscaping; see Appendix A
	Sidewalk	6' wide minimum
	Drainage Type	Per existing City of Lancaster requirements
Private Frontage		
G	Frontage Type	Buildings may front or side onto paseos; See Section 4.7

4.10.12 Fire Lane/Alley

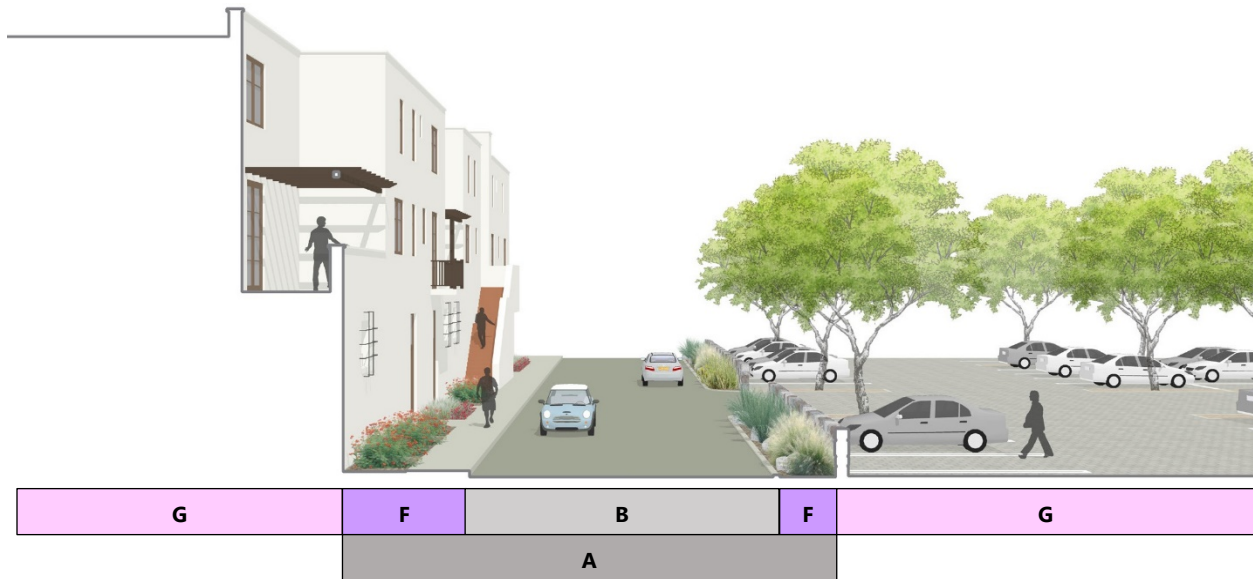


Figure 4.18, Fire Lane

Intent

Providing convenient customer access to the parking lots for the neighborhood shops and restaurants from commercial streets and emergency access for firefighters and other emergency responders. Service/fire lanes run along the backs of neighborhood shops and through the shared parking areas. A main water line runs along this lane bringing water to support fire suppression. This lane also serves as the primary route for delivery trucks, trash collection, and other service functions.

Alley travel through the interior of the bloc, providing rear access to buildings, whether commercial or residential, at a variety of scales. Alleys and Fire Lanes are inherently back-of-lot thoroughfares. Neither alleys or fire lanes may be fronted onto, meaning that buildings may have back access, but should not have primary access directly off of these thoroughfares, and that building faces at the rear are not required to create active frontages

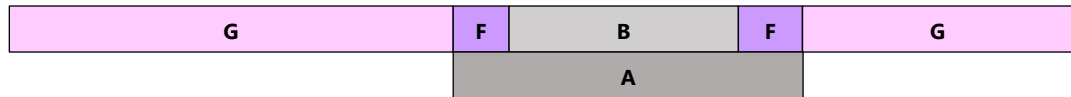


Figure 4.19, Service Alley

Design Standards		
Public Right-of-Way (ROW)		
A	Right-of-Way	Fire Lane: 34' minimum Alley: 20' minimum
	Curb-to-Curb	Fire Lane – 26'-28'; Alley – 20'
B	Travel Lanes	2 (1 each way); Fire lane: 13' lane width Alley: 10' lane width
C	Median	None
D	Bicycle Facilities	None
Public Frontage		
E	Flex Curbside Zone	No parking
F	Landscape/ Furniture zone	Not required; 2'-10' wide; street trees and native landscaping; see Appendix A
	Sidewalk	4' wide minimum Fire Lane: only required on building side Alley: Not required
	Drainage Type	Pervious paving; drains to center; or reverse crown
Private Frontage		
G	Frontage Type	Buildings shall not front onto Lanes/Alleys
Note: All alleys must be designed to meet Los Angeles County Fire Department Standards.		

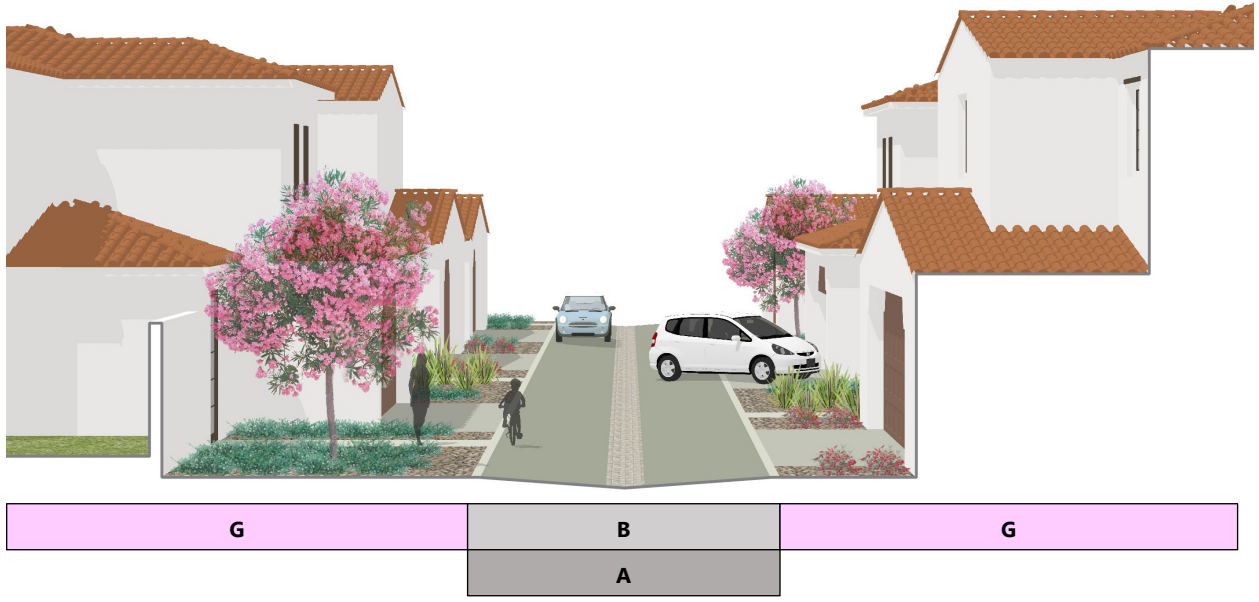


Figure 4.20, Residential Lane

4.11 Public Open Space

4.11.1 Introduction

High quality open spaces are a top priority of the Lancaster Health District Master Plan, and are crucial to the concerted strategy to make the Lancaster Health District an active, pedestrian-oriented district. As shown in Figure 4.11, the Lancaster Health District Master Plan includes an open space network of parks, greens, paseos, and squares. A central piece of the Public Open Space network, which provides a pedestrian-oriented anchor at the heart of the Health District, is Wellness Square, located adjacent to the new Antelope Valley Hospital and surrounded by 2- to 5-story medical offices, clinics, and wellness-education facilities. The square creates a prominent address for the new hospital at the crossroads of the primary north-south and east-west “medical mainstreets” of the District, providing good visibility and clear, direct access from 15th Street West, 20th Street West, Avenue J, and Avenue J-8. Other forms of open space are distributed throughout the blocks and neighborhoods as public amenities.

This section establishes the variety of Public Open Space types and their standards needed to implement the intended community form and neighborhood character described in Chapter 3.

4.11.2 Applicability

The standards of this section apply to new developments on sites of at least 20,000 square feet. The conceptual locations of public open spaces are shown in the Regulating Plan, Figure 4.11. The parcels containing Public Open Space in the Plan are required to provide the identified Public Open Space; exact location and dimensions may vary from the Plan drawing.

4.11.3 Standards & Guidelines Common to All

This section establishes the standards for the types of public open space based on the intended physical character through four types of spaces:

- Wellness Square
- Neighborhood Green
- Paseo
- Pocket Park

These types are generally based on their function. The Wellness Square is tied to the location of the new hospital. Paseos simultaneously count as public open space and thoroughfares (Section 4.10). Collectively, the spaces are designed to function as a

comprehensive system of public amenities and gathering spaces that supports the variety of physical contexts in mixed-use places.

The following standards regulate the size and design of each public open space to ensure that the intended form and function is achieved. See Appendix A, Landscape Guidelines, for tree, plant, lighting and street furniture guidelines.

4.11.4 Public Open Space Types

**Table 4.29
Public Open Space**

Open Space Types		Size		Reference
		Min	Max	
Wellness Square	(Area)	1.5 ac	2.0 ac	See Section 4.11.4A
Neighborhood Green	(Area)	0.25 ac	0.5 ac	See Section 4.11.4B
Paseo	(Width)	10'	--	See Sections 4.10.11, 4.11.4C
	(Depth)	--	300'	
Pocket Park		--	0.25 ac	See Section 4.11.4D

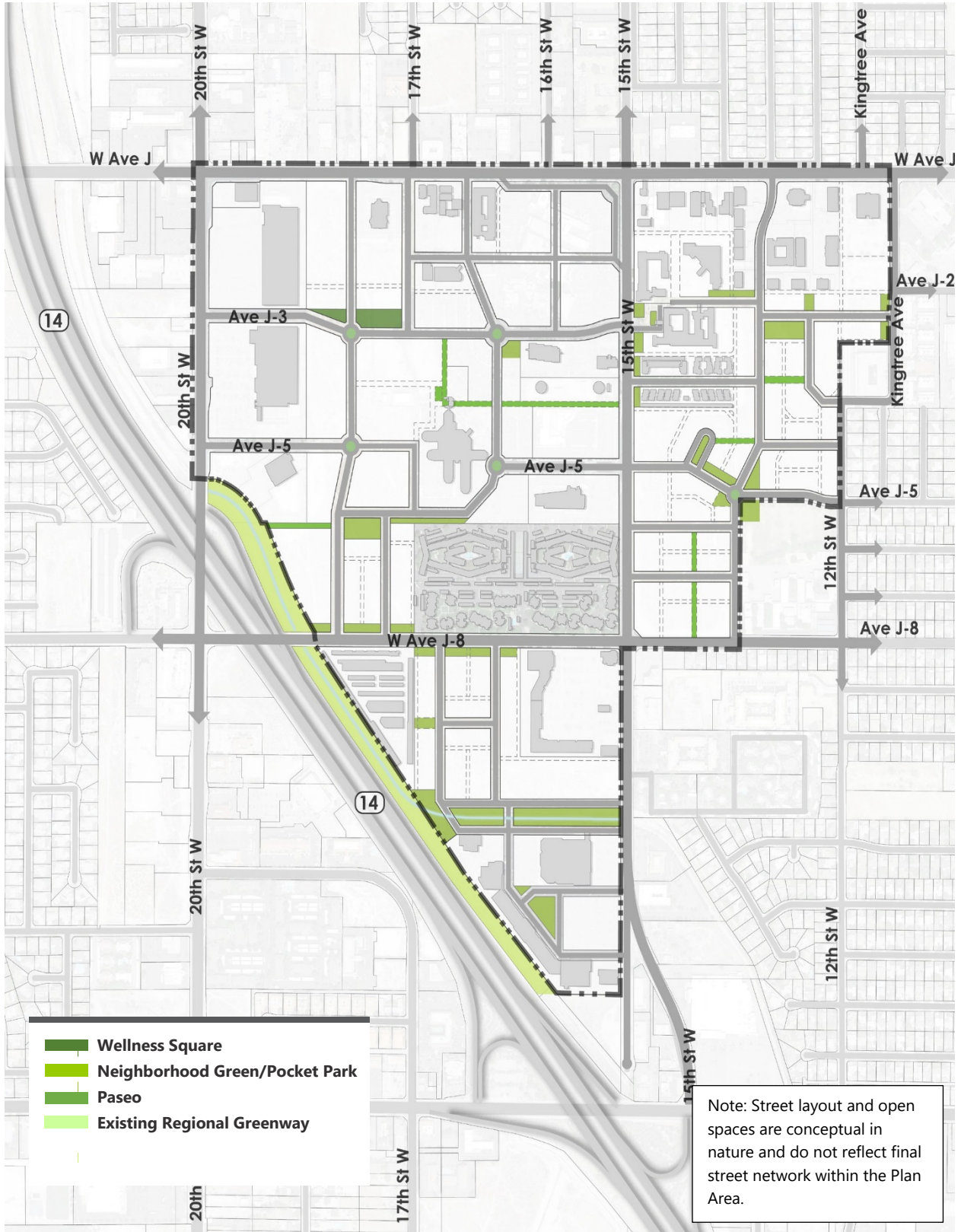
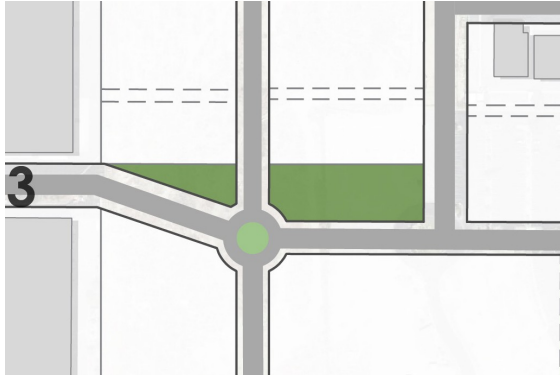


Figure 4.21, Open Space Regulating Plan

Wellness Square

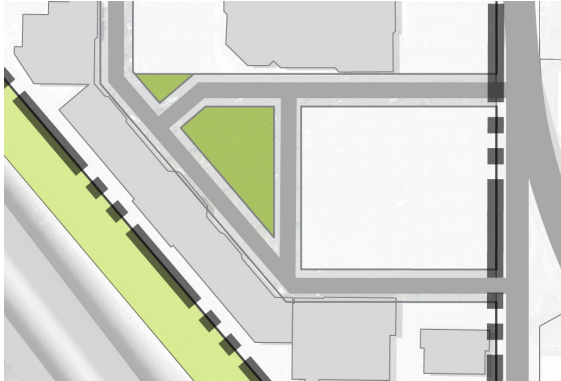
A formal area with focused landscaping and hardscape for civic purposes and commercial activities, spatially defined by building frontages, and located at the intersection of important streets of pedestrian paths.



Standards	Min	Max
Size (acres)	1.5	2
Additional Requirements		
<ul style="list-style-type: none"> Context. The square is defined on all sides by active ground floor frontages that provide space-activating amenities such as outdoor seating and dining. Crosswalks are required at all intersections at nodes of the square. Functional Design. Aside from its addressing and wayfinding role, the Plaza is intended to be the "outdoor living room" of the District and function as the center of community life. Visibility. No structures shall be placed in the square that will obstruct visibility from one side to the other. Shading. Galleries and arcades provide shaded, wind-protected pedestrian access so that public events hosted within those buildings can spill into the square. Moveable furniture within the plaza will enable people to seek shade of sun while spending time eating, visiting, and relaxing. Lighting, Structures and Improvements. The landscape design of the square involves shade trees along the north and south edges, with large open turn in the center for staging of events. The surrounding buildings and trees will provide shelter from the sun and wind. 		

Neighborhood Green

A neighborhood green is a small open space within a neighborhood, spatially defined by building frontages and streets and accommodating passive recreation and children’s play. While greens may include playgrounds, they are primarily intended as informal spaces with no dedicated recreational use.

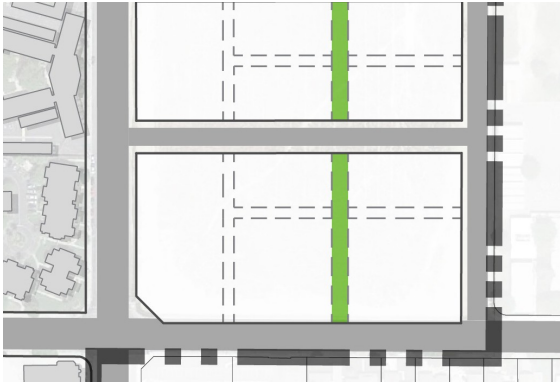


Standards	Min	Max
Size (acres)	0.25	0.5
Additional Requirements <ul style="list-style-type: none"> • Context. Adjacent homes shall front onto attached greens. Greens shall have at least two streets as boundaries, and may be within or at the end of residential blocks. Attached greens shall be part of the block, with no vehicular right-of-way between the green and adjacent residential lots, while unattached greens may have a vehicular right-of-way that separates the green from residential lots. • Functional Design. Landscape shall consist of unobstructed lawn, planting beds, trees and/or areas of drought tolerant landscape. Trees shall be arranged naturally and provide shade. Hardscape is only in support of access to sidewalks and peripheral connections. • Visibility. Hedges and walls within the green are limited to 36 inches in height. • Shading. Drought tolerant trees provide majority of shading. Shades structures are permitted but should be kept to a minimum. • Lighting, Structures and Improvements. Built structures may include pergolas, trellises, small monuments, water features, and pedestrian amenities (benches, tables, drinking fountains, etc.), bike racks, playground equipment and informal athletic courts. Adequate lighting shall be provided. 		



Paseo

A paseo is a narrow pedestrian way that provides mid-block connections. A paseo is especially useful in blocks with large perimeters. Paseos may exist in residential or commercial blocks.



Standards	Min	Max
Width	10'	--
Depth	--	300'

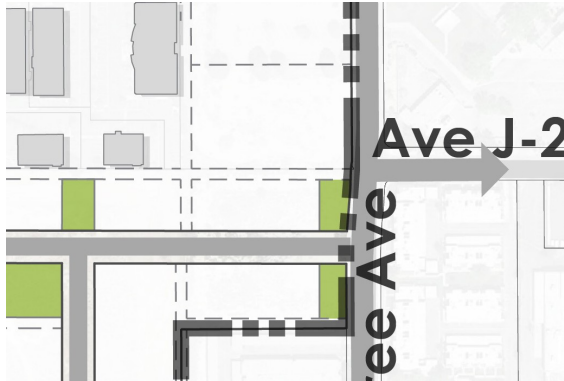
Additional Requirements

- Context. Paseos may be used in any zone and can accommodate residential or non-residential frontages. While there are no required locations for paseos, they present the opportunity to improve pedestrian connectivity. When possible, paseos should lead to other open spaces.
- Functional Design. Paseos should be used to break up lengths of blocks in order to improve walkability. In residential areas, paseos create open spaces that support passive recreational activities as well as convenient connections to the rest of the neighborhood. In commercial areas, paseos may provide additional locations for store frontages, patios and outside dining, informal open spaces and mini plazas between buildings. Paved paths and hardscape features should be integrated with landscaped lawns and planting beds to encourage pedestrian movement.
- Visibility. Entrances are placed directly on major right of ways, with landscape buffers restricting vehicular access.
- Shading. Paseos shall be appropriately placed and scaled to allow significant daylight and shading.
- Lighting, Structures, and Improvements. Small semi-public side courts and pedestrian amenities (benches, picnic tables, etc.) may be provided. Adequate lighting shall be provided.



Pocket Park

A pocket park is a small open space tucked into a block for the recreation of children, generally fenced and may include an open shelter. Pocket parks may also be designed primarily for quiet, passive recreation, and in all cases, they are intended to be interspersed within neighborhoods.



Standards	Min	Max
Size (acres)	--	.25
Additional Requirements		
<ul style="list-style-type: none"> Context. Pocket parks should be located on corners. They should be strategically placed and sized to contribute to the larger network of open spaces. Pocket parks are often ideal on small neighborhood blocks adjacent to blocks that have larger open spaces. Pocket parks should be located on block corners. Functional Design. A pocket park is a flexible community gathering space and can be an iconic market for smaller neighborhoods. Groundcover may range from predominantly green, to hardscape, or a balance of both. Unobstructed lawn, and/or drought tolerant landscape is recommended. Visibility. Hedges and walls within the pocket park are limited to 36 inches in height. Shading. Shade structures may be allowed but are limited in use and appropriately scaled. Lighting, Structures, and Improvements. Built structures may include pergolas, trellises, water features, and pedestrian amenities (benches, tables, drinking fountains, etc.), bike racks, playground equipment and informal athletic courts. Adequate light shall be provided. 		

